

DESIGN REVIEW BOARD MEETING

CITY OF DAVENPORT, IOWA

Monday, March 23, 2026; 5:00 PM

Davenport Police Department | 416 N Harrison Street | Community Room

I. Call to Order

II. Secretary's Report

1. Consideration of the November 24, 2025 meeting minutes.

III. Old Business

1. Case DR25-08: Request for Design Review, C-V Village of East Davenport Zoning District; revision on South elevation for facade improvements to The Village Theatre located at 2113 East 11th Street. Streamline Architects, petitioner, on behalf of Heights Holdings LLC. [Ward 5]

IV. New Business

1. Case DR26-01: Request for Design Review, C-E Elmore Zoning District; construct Stuff Etc. located at Jersey Farms Commercial Park 2nd Addition Lot 1. MGNS LLC petitioner. [Ward 8]
2. Case DR26-02: Request for Design Review, C-D Downtown Zoning District; build flood protections for Union Station. 102 S. Harrison St. City of Davenport, petitioner. [Ward 3]
3. Election of Chairperson
4. Election of Vice-Chairperson

V. Public Comment

VI. Adjournment

VII. Next Meeting: April 27, 2026

City of Davenport

Department: Development & Neighborhood Services
Contact Info: Matthew Reu | 563-888-2221

Action / Date
3/23/2026

Subject:
Consideration of the November 24, 2025 meeting minutes.

Recommendation:
Approve the minutes.

Background:
The November 24, 2025 meeting minutes are attached.

Attachments:
1. Meeting Minutes 11-24-25

MINUTES
DESIGN REVIEW BOARD MEETING
CITY OF DAVENPORT, IOWA
MONDAY, NOVEMBER 24, 2025; 5:00 PM
CITY HALL | 226 WEST 4TH STREET | COUNCIL CHAMBERS

I. Call to Order

Present: Tebbitt, Hoff, Paone, Tylka, Anderson
Excused: Molacek, Stinocher, Martin, Rashid
Staff: Reu, Werderitch

II. Secretary's Report

1. Consideration of the August 25, 2025 meeting minutes.

Motion by Tylka, second by Hoff to approve the August 25, 2025 meeting minutes.
Motion to approve was unanimous by voice vote (5-0).

III. Old Business

IV. New Business

1. Case DR25-14: Request for Design Approval, C-D Downtown Zoning District; Construction of a dumpster enclosure at 324 East River Drive. Downtown Davenport Partnership, petitioner. [Ward 3]

Staff provided an overview of the request. The Downtown Davenport Partnership is petitioning construction of a dumpster enclosure near the corner of Iowa Street and River Drive to enhance the beautification of Motor Row Alley, combine existing alley dumpsters, and reduce litter issues in the corridor. The dumpsters will be enclosed with a six-foot-tall composite wood fence. The gate will be a black-coated chain link fence with privacy slats.

Staff recommended Case DR25-14 be approved in accordance with the submitted work write-up and materials.

Kyle Carter, Downtown Davenport Partnership, was in attendance to answer questions and further describe the improvements to Emerson Alley.

Board members inquired about the placement of the dumpster on the property, and future plans for Motor Row & Bucktown Beautification Project.

Motion by Tylka, second by Tebbitt, to approve Case DR25-14 in accordance with the submitted work write-up and materials. Motion was approved by a roll call vote (5-0).

V. Public Comment

VI. Adjournment

Motion by Tebbitt, Second by Tylka, to adjourn the meeting. Meeting adjourned at 5:12pm.

VII. Next Board Meeting: December 22, 2025

City of Davenport

Department: Development & Neighborhood Services
Contact Info: Matthew Reu | 563-888-2221

Action / Date
3/23/2026

Subject:

Case DR25-08: Request for Design Review, C-V Village of East Davenport Zoning District; revision on South elevation for facade improvements to The Village Theatre located at 2113 East 11th Street. Streamline Architects, petitioner, on behalf of Heights Holdings LLC. [Ward 5]

Recommendation:

Staff recommends the amendment for Case DR25-08 be approved in accordance with the submitted work write-up and materials.

Background:

The applicant is changing their plans from the previously approved work presented in March, 2025. Staff do not object to the french doors and 3 x 4 grid windows with black trim. Based on the historic photo submitted, there did not appear to be any grids on the South facing windows. However, staff are not opposed to the 3-over-4 grid configuration as it adds character to the facade and matches the french doors. Staff advise that the grid pattern be external to better replicate historic depth and dimensions. Grids located internally within the glass are often flush and lack the historic charm of original wood windows.

The following scope of work is proposed:

1. Install 6" fiber cement lap siding.
2. Paint siding as Garden Sage.
3. Install black guard rails and black ornamental fencing.
4. Install windows with 3-over-4 black grid.
5. Install three black framed french doors.
6. Infill existing window with block, paint to match siding.
7. Install uplighting near foundation.

Village of East Davenport Performance Standards & Design Guidelines:

The design guidelines recommend the following standards for remodeling existing buildings:

1. Preserve significant stylistic and architectural features.
2. Repair deteriorated building features.
3. Replace an architectural element accurately if it must be replaced, matching the material and design.
4. Do not use synthetic materials, such as aluminum, vinyl or panelized brick/stone as replacement for primary building materials.

Previous Case 03/24/2025:

The East Davenport Turnverein (Turner) Hall, currently known as The Village Theatre, is listed

as a contributing structure in the Village of East Davenport Historic District. The structure was built in 1909 and served as a prominent meeting space for the German community.

Design Request:

Heights Holdings LLC purchased The Village Theatre in late 2024 for the purpose of using the historic building as a community center. The intent is to renovate the exterior to better replicate the historic architectural features.

The following scope of work is proposed:

1. Replace existing exterior siding with fiber cement siding. Current siding is vinyl.
2. Replace windows with black windows and trim.
3. Replace asphalt shingle roof with a charcoal standing seam metal roof.
4. Modify north elevation roof peak to be curved to match historic photo.
5. Replace north entrance canopy with new to match historic photo.
6. Add concrete terrace on south elevation at height of existing stage.
7. Add new door, sidelights and transom on south elevation to terrace.
8. Replace concrete south of new terrace to existing curb with stamped concrete to match brick pavement to the west.
9. Replace parapet at south entry door with curved parapet, add awning above door.

Staff Review:

A historic photo of Turner Hall has been submitted as a reference and inspiration for the restoration. The renderings indicate returning the north elevation roof pitch to a rounded arch. In addition, the north entrance canopy will be removed and reconstructed with a flat roof and colonial-style columns (painted white). Staff are supportive of the architectural changes to the north facade as it removes previous alterations to the building that were incompatible with the original design. The proposed structural alterations better reflect the historic character of the building.

The replacement siding product is James Hardie, which is a fiber cement product. There are several buildings within the Village of East Davenport with James Hardie siding or other similar composite siding materials. The most recent example is at Wide River Winery. James Hardie is also a high-quality siding product that can replicate a wood finish. According to the historic property inventory sheet, the original siding material was wood. However, the building has since been resided with a cream/yellow vinyl siding that is starting to show some deterioration. The replacement siding color is 'Boothbay', which will coordinate well with the recently renovated restroom building in Lindsay Park.

Staff do not object to the replacement double-hung windows with black trim. Based on the historic photo submitted, there did not appear to be any grids on the north facing windows. However, staff are not opposed to the six-over-six grid configuration as it adds character to the facade. Staff advise that the grid pattern be external to better replicate historic depth and dimensions. Grids located internally within the glass are often flush and lack the historic charm of original wood windows.

The existing roofing material on the Village Theater is an asphalt shingle product. Staff do not object to the proposed charcoal standing seam metal roof as the product is highly durable and

will keep the structure in a good state of repair. An alternative plan, if the metal roof is cost-prohibitive, is to replace the roof with asphalt shingles in a similar charcoal color. Staff are supportive of both options.

The footprint of the concrete terrace on the south elevation extends into property owned by the City of Davenport. Therefore, approval from City Council is required prior to any construction. The Design Review Board can review the proposal for conformance with the Village of East Davenport Performance Standards & Design Guidelines. If the applicant is unable to acquire the property or obtain approval from City Council, the applicant can return to the Board with a revised design for the south elevation.

Contingent upon City Council approval, staff have no objection to the proposed materials, which include a black ornamental railing, decorative light posts, a stone texture on the wall base, replacement windows, and a new door with sidelights and transom. Staff recommend matching the decorative lighting with the fixtures in Lindsay Park, directly east of the Village Theatre. In general, the proposed materials are consistent with those utilized throughout the Village of East Davenport.

The renderings indicate that the walkway adjacent to the parking lot will be finished with stamped concrete to resemble the brick pattern in front of the Village Market Place. This is considered an improvement since the existing area is currently deteriorated asphalt. However, staff recommend the width be increased to 5 feet to be more accommodating to pedestrians utilizing the adjacent accessible parking spaces.

There is a narrow corridor between the Village Theatre and the Village Market Place. The two buildings are connected by a wall with green wood siding and a recessed doorway. The applicant is proposing to reconstruct this space with a rounded parapet arch, curved entrance awning, and faced with matching fiber cement siding. Staff believe the intent is to replicate the rounded arch on the north-facing facade. Since this architectural feature serves as a transition between the two structures, careful consideration is merited by the Board.

Once improvements to the south elevation are completed, the exterior masonry will be painted in a similar Boothbay color to compliment the James Hardie siding. The Design Review Board should consider how this color interacts with the adjacent Village Market, which has green masonry and red window trim.

Village of East Davenport Performance Standards & Design Guidelines:

The design guidelines recommend the following standards for remodeling existing buildings:

1. Preserve significant stylistic and architectural features.
2. Repair deteriorated building features.
3. Replace an architectural element accurately if it must be replaced, matching the material and design.
4. Do not use synthetic materials, such as aluminum, vinyl or panelized brick/stone as replacement for primary building materials.

Attachments:

1. 2026 Application
2. 2026 Rendering of the Village Theater
3. Former March 2025 Village Theater Renderings
4. 2025 Photos of the Village Theatre
5. Historic Property Inventory Sheet-Village Theatre
6. Village of East Davenport Performance Standards and Design Guidelines



CITY OF DAVENPORT
 Development & Neighborhood
 Services – Planning
 1200 E. 46th St
 Davenport, IA 52807

Office 563.326.6198
 planning@davenportiowa.com

APPLICATION FOR
DESIGN REVIEW
 DESIGN REVIEW BOARD

REVIEW DISTRICT	SUBMITTAL DATE	MEETING DATE

SITE ADDRESS & PARCEL OR LOCATION DESCRIPTION

APPLICANT INFORMATION

Applicant Name | Company Name
 Address
 City | State | Zip
 Phone
 Secondary Phone
 E-Mail Address

BRIEF OVERVIEW OF THE PROJECT (not a scope of work)

APPLICABILITY (PRIOR TO ANY WORK, APPROVAL IS REQUIRED)
Design Review Applications must be completed and approved PRIOR to the commencement of the following types of work:

- New construction or an alteration to an exterior of a structure where changes are visible from the public right-of-way
- New parking lots, fencing/walls, landscaping, or alterations of
- Sign installation or alteration
- Streetscaping element installation within the public right-of-way

Acceptance of Applicant
 I, the undersigned, certify that the information on this application to the best of my knowledge is true and correct. I further certify that I have a legal interest in the property in question, and/or that I am legally able to represent all other persons or entities with interest in this property, and acknowledge formal procedure and submittal requirements.
 I understand I am responsible for attendance at the meeting as shown on the design review board calendar. The City reserves the right to require further site studies as necessary.
 By checking this box and typing my name below, I am electronically signing this application.

_____ Date _____
 Type Applicant's Name here to serve as a signature

ALL SUBMITTALS SHALL INCLUDE: **SUBMITTED**

Full Scope of Work (SOW) attached as a .PDF
 all work & materials shall be described & itemized/listed in detail
 Digital photos of existing building/sign elevations/façades
 Proposed color building/sign elevations to scale
 rendering as proposed & depicted on existing building/signs
 Material specifications: type, dimensions, & color
MINOR & MAJOR ADDITIONS, SITE IMPROVEMENTS, & NEW BUILDINGS SHALL INCLUDE ADDITIONAL ITEMS*:
 Dimensioned Site Plan (proposed & existing buildings/site items)
 Landscape Plan
 Grading Plan with 2 foot intervals (if needed)
 Storage & Mechanical Screening shall be shown

* Major Additions & New Buildings may require more extensive information

DEVELOPMENT TEAM

Property Owner

Address
 Phone Secondary Phone
 E-Mail Address

Formal Procedure Application Fee: **NONE**

(1) Application:
 ● Prior to submission of the application, the applicant shall correspond with Planning staff to discuss the request, potential alternatives and the process.
 ● The submission of the application does not constitute official acceptance by the City of Davenport. Planning staff will review the application for completeness and notify the applicant that the application has been accepted or additional information is required. Inaccurate or incomplete applications may result in delay of required regularly scheduled meetings.

(2) Scope of Board's Consideration:
 ● Only work described in the application may be approved.
 ● If insufficient information exists to make a proper judgment on the application, the Board may continue the consideration a maximum of three regularly scheduled consecutive meetings, excluding applicant requested continuances.

(3) Post Board Ruling:
 ● If approved, design approval does not constitute a City permit or license and does vest against any other land development regulation or regulatory approval. Applicant must contact necessary development authorities.
 ● Design approval expires one year from the date of approval unless a building permit is obtained within such period. An applicant may apply in writing for an extension of time at any time prior to the date of expiration.
 ● Appeals to the Board's determinations are \$75 made to City Council and shall be in writing submitted to the Zoning Administrator within 30 calendar days of Board's decision.

Project Manager/Other

Address
 Phone Secondary Phone
 E-Mail Address

Submit this form with attachments to: planning@davenportiowa.com

Design Review Board (DRB) | City of Davenport Meeting Calendar 2026

The Applicant and/or their representative MUST attend the Meeting

Submittals and certifications are to be submitted to Davenport Public Works, 1200 E 46th St, Davenport, IA 52807.

Submittal/Application Deadline**	Meetings***
12:00 P.M. on the Friday Identified Below	5:00 P.M. on the Monday Identified Below Applicant/Representative Presence Required
01/16/26	01/26/26
02/13/26	02/23/26
03/13/26	03/23/26
04/17/26	04/27/26
05/15/26	05/26/26
06/12/26	06/22/26
07/17/26	07/27/26
08/14/26	08/24/26
09/18/26	09/28/26
10/16/26	10/26/26
11/13/26	11/23/26
12/11/26	12/21/26

* Change Due to Holiday.

** Email completed PDF Application to: planning@davenportiowa.com, or mail USB drive with a copy of the completed application to: Planning, Davenport Public Works, 1200 E 46th St, Davenport, IA 52807.

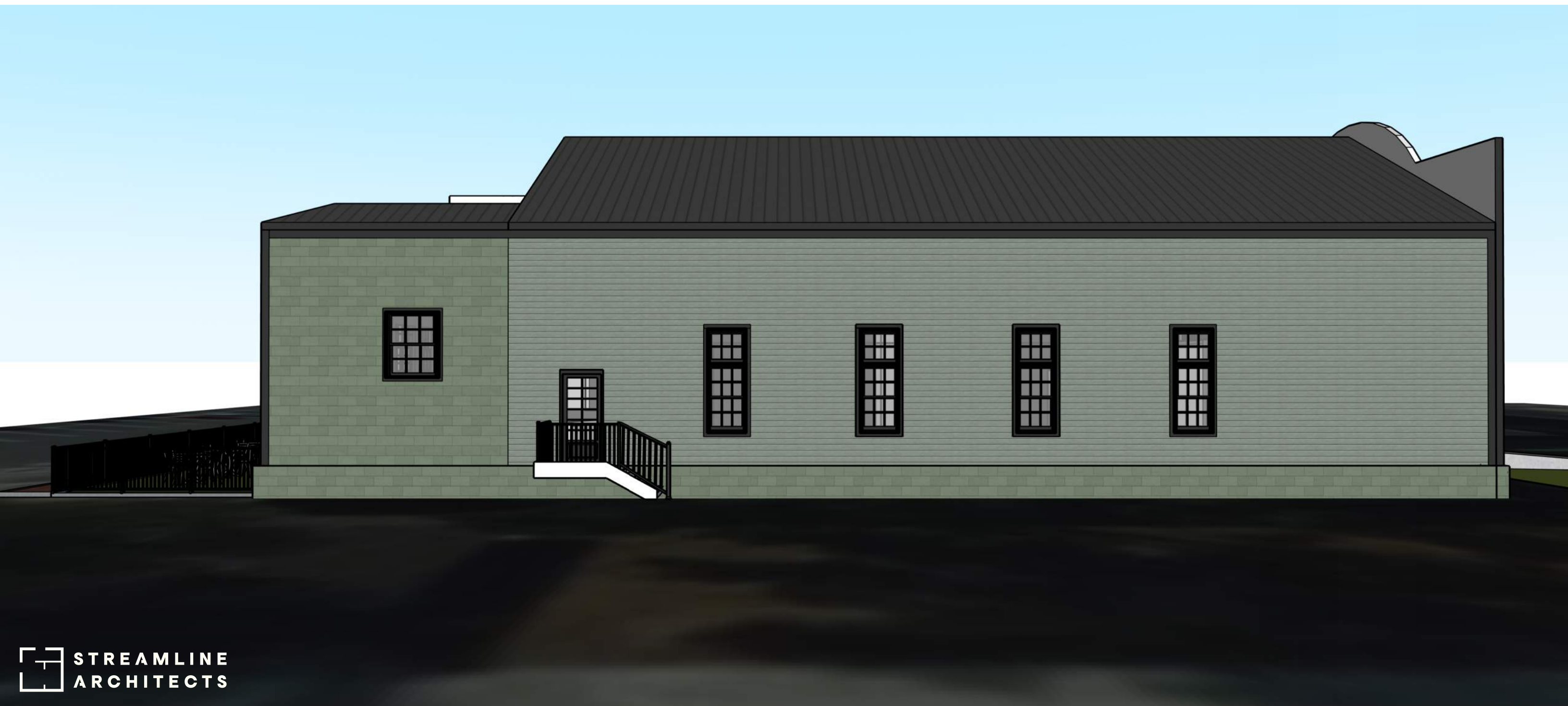
*** All meetings are held at Davenport City Hall, 226 W 4th Street, Davenport, IA.
Applicant/Representative Presence Required

For more information contact: planning@davenportiowa.com or 563.326.6198.

Any and all Date/Location/Time are subject to change.

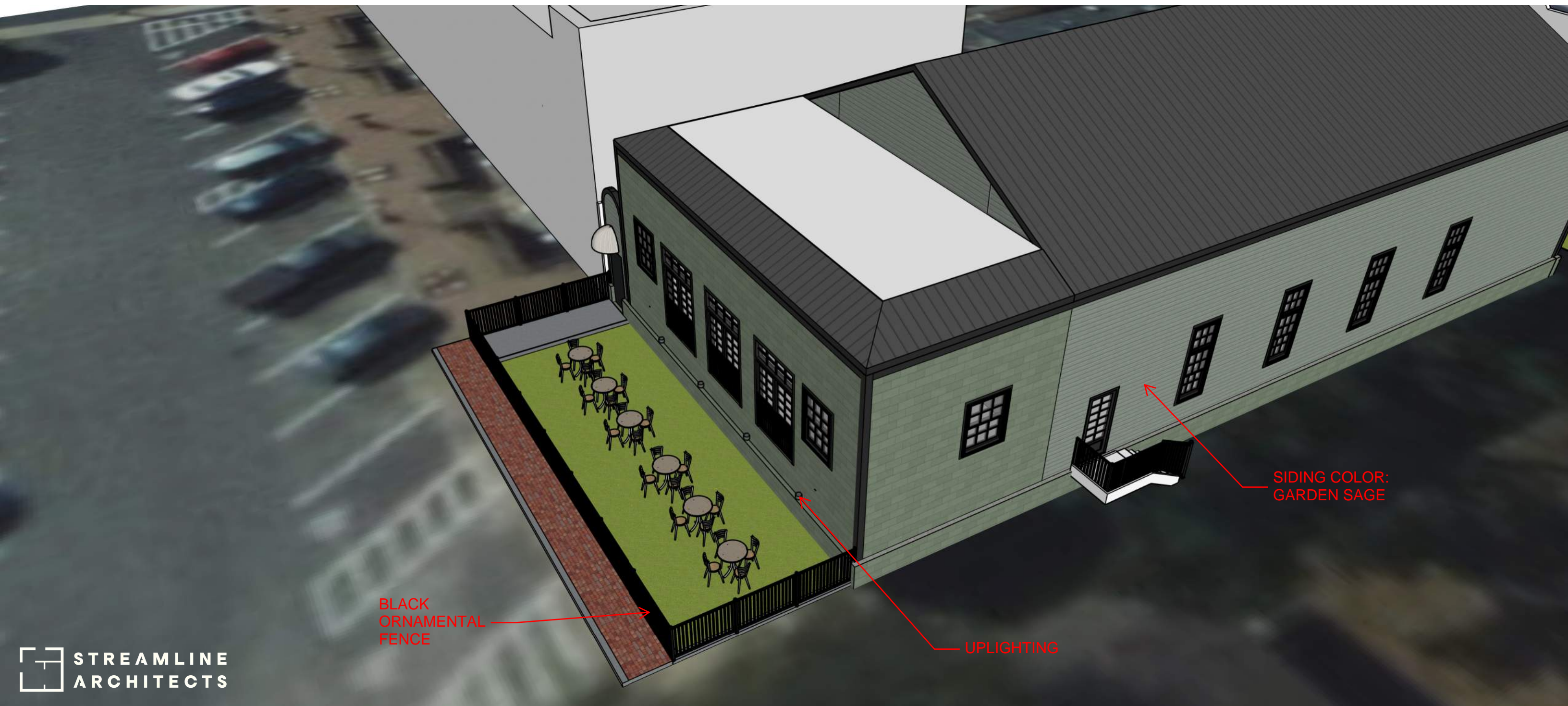












BLACK
ORNAMENTAL
FENCE

UPLIGHTING

SIDING COLOR:
GARDEN SAGE



PAINT: GARDEN SAGE
TO MATCH SIDING

FRENCH DOORS - BLACK

BLACK ORNAMENTAL
GUARDRAIL

STANDING SEAM
METAL ROOF -
CHARCOAL

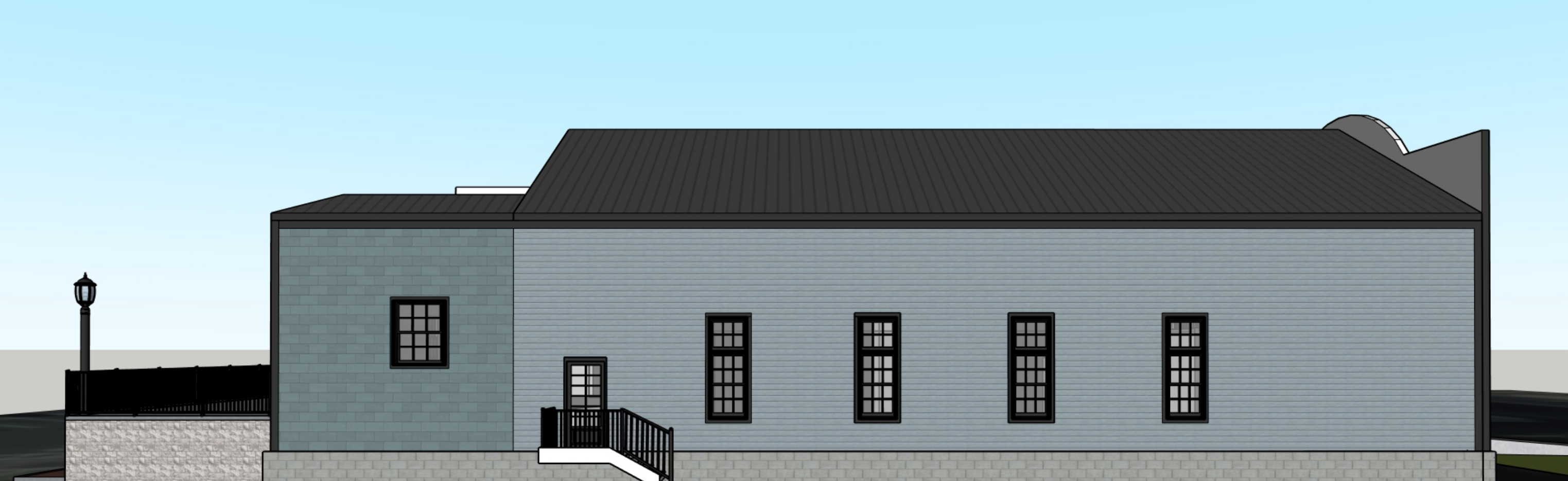
REVISE PARAPET
TO BE CURVED

BLACK WINDOWS &
FRAMES, TYP.

6" FIBER CEMENT LAP
SIDING - JAMES HARDIE
"BOOTHBAY"

COLONIAL
COLUMNS AND
CANOPY - "WHITE"





BLACK
ORNAMENTAL
RAILING



DECORATIVE LIGHT
POLE - BLACK

CAST-IN-PLACE
CONCRETE WALL -
STONE TEXTURE

PAINT EXISTING
MASONRY - COLOR
TO MATCH SIDING



REVISE PARAPET
TO BE CURVED

CURVED
ENTRANCE
AWNING

NEW DOOR WITH
SIDELIGHTS AND
TRANSOM - BLACK

STAMPED
CONCRETE - BRICK
PATTERN TO
MATCH EXISTING

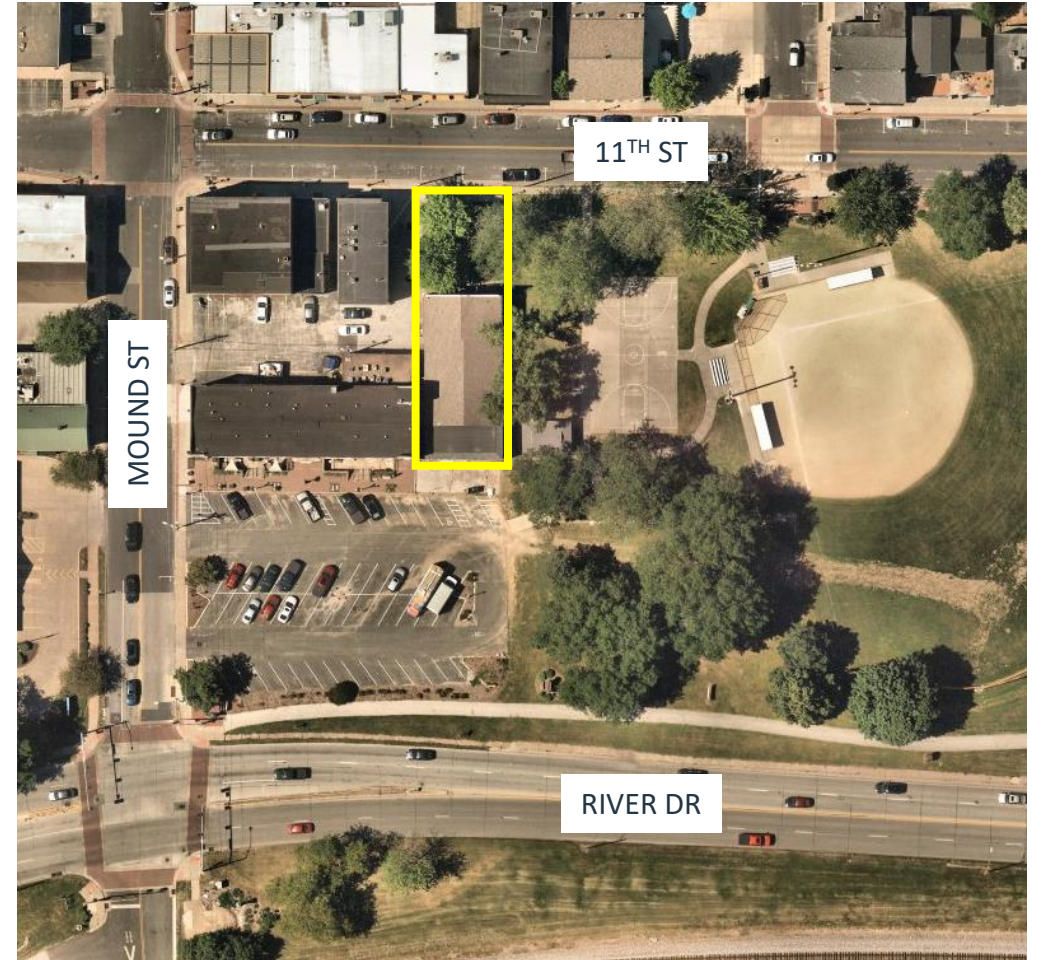




EAST DAVENPORT TURNER HALL

The Village Theatre

DAVENPORT
IOWA | USA



The Village Theatre

DAVENPORT
IOWA | USA



The Village Theatre

DAVENPORT
IOWA | USA



The Village Theatre

DAVENPORT
IOWA | USA



The Village Theatre

DAVENPORT
IOWA | USA



The Village Theatre



The Village Theatre

DAVENPORT
IOWA | USA



Lindsay Park

DAVENPORT
IOWA | USA



The Architects Office

Wehner, Nowysz, Pottschiull and Pfiffner
201 day building, Iowa city, Iowa 52240

DAVENPORT COMMUNITY DEVELOPMENT DEPARTMENT
IOWA DIVISION OF HISTORIC PRESERVATION

SITE #82-10-11-E2113 MAP #

HIST. DIST. East Davenport

NAME East Davenport Turnverein Hall (H) C

ADDRESS 2113 E. 11th Street

Pt. of Sec 30-78-4 Com 130' E of SE cor Mound & Eddy Sts
S 150' - E 49' - N 150' - W to Beg (Exc. part exempt)

LEGAL DES. SUB-DIVISION BLOCK PARCEL SUB-PARCEL

UTM 15 704760 4600380 ACREAGE -1 ZONE C-2
EASTING NORTHING

OWNER East Davenport Turner Verejn

TITLE H. East Davenport Turner Verejn
(IF DIFF) 2113 E. 11th St.
Davenport, IA 52803



MAP

SITE SHEET

DESCRIPTION

FORM 2 story auditorium, gable roof with round-arched front parapet CONST. 1909 & later

MATERIALS wood ARCH STYLE

FENESTRATION Rectangular, varying size

DIST. FEATURES

ALTERATIONS

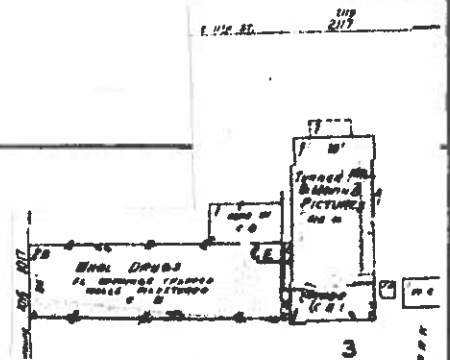
SITE & RELATED STR. at edge of grassy park & playing fields, across street from small commercial district

ARCHITECTURE

STATEMENT

Contributing structure in E. Davenport Historic District.

SOURCES



ADDENDUM No. 1

**Village of East Davenport Performance Standards
and Design Guidelines**

**Endorsed by the City of Davenport
Design Review Board**

February 24, 2014

5.0 BUILDING DESIGN & LAND USE



Appropriate building design in the Village is important in order to retain the character of the area and promote the community vision for the Village of East Davenport as a regional tourist destination and attractive neighborhood center.

This chapter describes the land use regulations that shape development in the Village and provides principles and guidelines for building design.



5.1 Land Use Regulations

The City's existing land use and zoning standards help shape development in the Village. They regulate permitted uses as well as site and building characteristics such as parking, height and setbacks. In many cases, existing land use and zoning standards are consistent with the community's vision for the Village. However, some adjustments may be necessary to better direct future development.

The Village core is zoned as Historic Shopping District (HSD), which is intended to:

- Preserve and enhance the smaller scale or historic character of older commercial areas.
- Stabilize and increase property values by discouraging incompatible development.
- Preserve unique historic and architectural character by controlling the design and scale of infill construction.
- Encourage heritage tourism.

Permitted Uses

Uses permitted in the HSD district include

- Multifamily residential
- Live/work units
- Bed and Breakfast establishments and small inns
- Bars and taverns
- Restaurants
- Stores and retail establishments of various kinds
- Professional services
- Salons and health spas

A number of uses are specifically excluded, including automobile dealerships, drive through sales and liquor stores (excluding wine shops). In addition, bars and taverns may not occupy more than 35% of each block face.

Additional uses that are not specifically listed as permitted in the HSD district may be appropriate to promote the community's vision. They include:

- Boutique hotels
- Grocery stores (gourmet and health food stores are currently permitted)

Design Standards

In most cases, the design standards that shape the development of sites and buildings in the Village are contextual. That is, they vary according to the surrounding context to help ensure compatible height, setbacks and width. Overall building heights may not, however, exceed three stories or 45 feet.

The existing design standards in the HSD district are generally consistent with the community's vision for the Village. In some cases, however, additional clarification or illustrations could be helpful. Additionally, it may sometimes be appropriate to allow building elements that are taller than three stories as part of larger developments that are not adjacent to lower scale neighborhoods or historic resources.

Design Review

New buildings, and exterior modifications, signs or additions are reviewed by a design review committee using adopted performance standards and design guidelines.

The design guidelines should be updated to more clearly articulate the community's vision for the Village, including mixed-use Village character, context-sensitive design guidance for specific subareas and appropriate historic rehabilitation. The general guidelines provided in this chapter provide a foundation for such an update.

5.2 Preserving a Sense of History

An urban village has layers of history. Historic buildings and sites should be preserved to contribute to the character of the street. Some will have compatible additions that reflect an evolution in uses.

The proper treatment of historic buildings will ensure that they continue to contribute to the historic ambiance of the Village. With careful treatment, the existing historic resources will continue to promote the unique atmosphere that makes the Village an inviting place to visit.

Guidelines:

- Preserve significant stylistic and architectural features and materials.
- Repair deteriorated building features.
- Replace an architectural element accurately if it must be replaced, matching the material and design.
- Avoid adding details that were not a part of the original building.

See “Heritage Resources” on page 57 for more information.



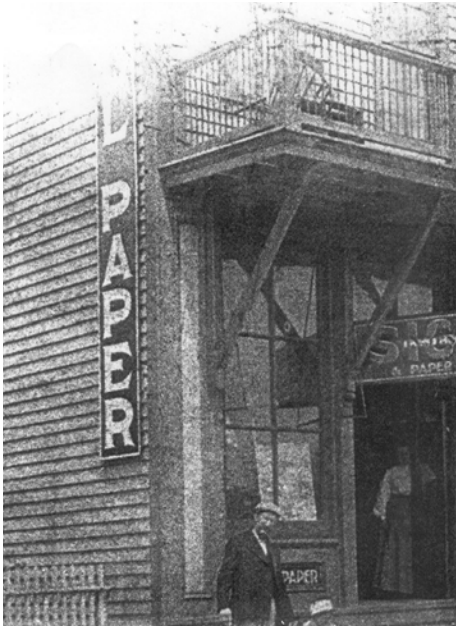
An urban village has layers of history. Historic buildings and sites should be preserved to contribute to the character of the street.

Potential Infill Standards

Although preservation of the Village’s historic buildings is a priority, it may sometimes be necessary to demolish an older building to make way for new development that is consistent with the community vision for the Village. The City should require an approved design for new development, or consider a demolition delay to provide time for consideration of other options, prior to issuing a demolition permit. The City should also consider design standards for new infill development to:

- Encourage high quality design and materials in new infill development
- Discourage parking areas located between buildings and the street
- Discourage parking as a primary use (properties use exclusively to provide parking)

Rehabilitation Opportunities



A historic image shows the storefront and balcony in a similar configuration as it looks today.



Preserve significant stylistic and architectural features and materials.

Guidelines:

- Do not use synthetic materials, such as aluminum, vinyl or panelized brick/stone as replacement for primary building materials.
- Do not cover original materials with new materials.
- Consider removing later covering materials that have not achieved historic significance.

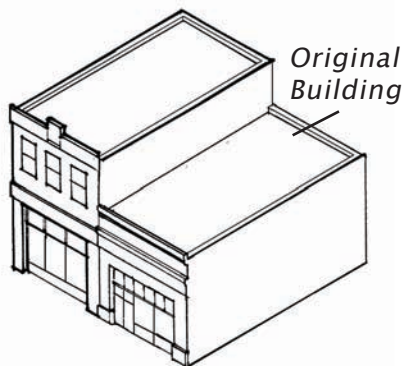


The rehabilitation of this building is expected to be completed in 2013.

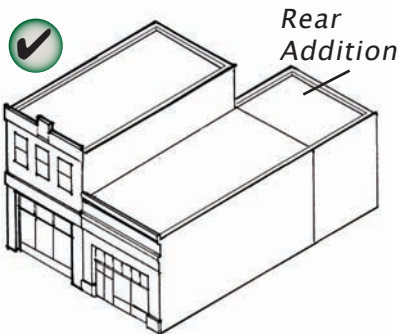
Locating a Commercial Addition

An addition to a historic building may be located to the rear or to the side or roof as illustrated below.

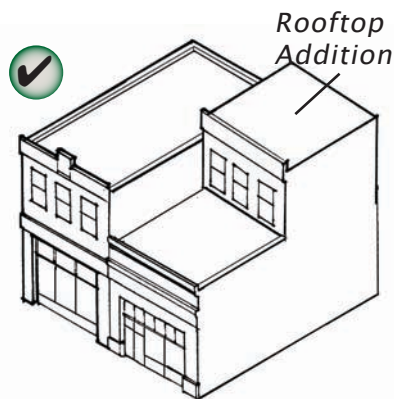
Original Building



New Addition to the Rear



New Addition to the Side and Roof Addition



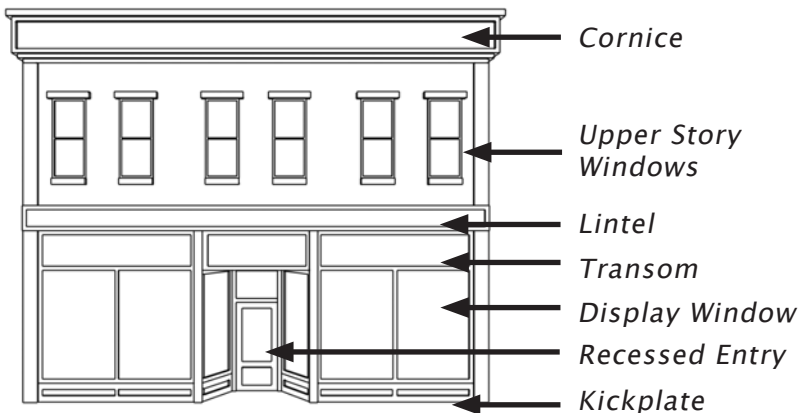
Guidelines:

- Preserve the position, proportion, and arrangement of historic windows and doors on a primary facade.

Special consideration should be given to the important historic elements of specific building types including historic commercial storefronts and residential buildings.

- Preserve the character-defining features of a historic commercial storefront. Typical features include the storefront itself as well as features on the upper stories of the building as illustrated below.

Character-Defining Elements of a Historic Commercial Storefront



Treatment of an Altered Historic Commercial Facade

The guidelines in this *Historic Commercial Properties* section discuss a range of treatment options for commercial facades, including reconstruction and replacement. When applied to a building that is already altered, which would be the best approach? The diagram below outlines the treatments to consider in making that decision. Treatment 1 is always the first priority.



Altered Historic Commercial Facade

Treatment 1: Reconstruct



When should I use this treatment?

- The building is highly significant.
- There is good historical information about the design.
- The needed materials and craftsmen are available.
- The context has many intact historic buildings.

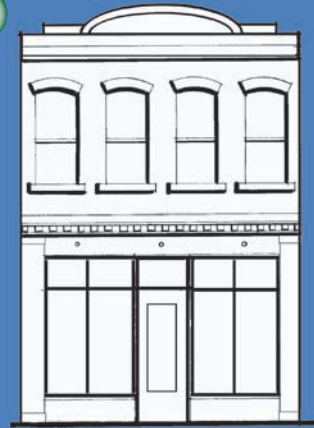
Treatment 2: Simplified Interpretation



When should I use this treatment?

- The building is a contributor to a district.
- There is less historical information available about the original design.

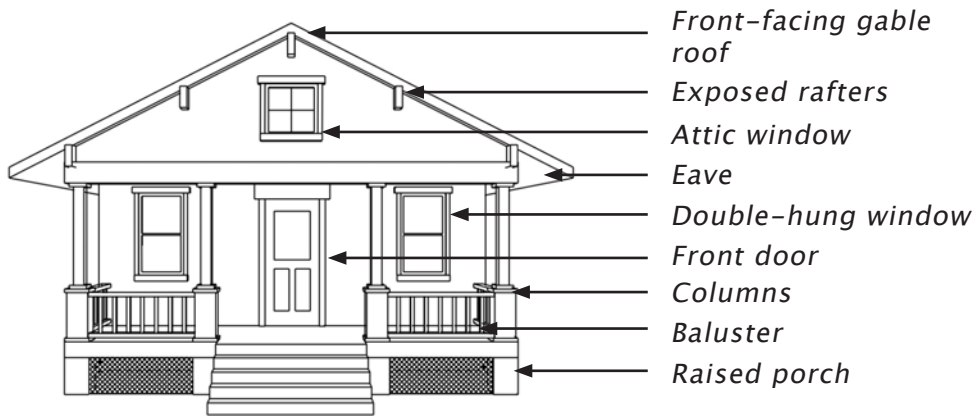
Treatment 3: Contemporary Interpretation



When should I use this treatment?

- There is substantial alteration, making "Treatment 1" difficult.
- There is less historic information about the original design.
- The context has more variety.

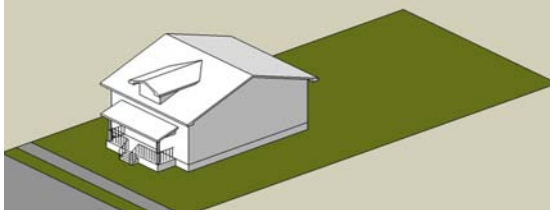
Character-Defining Elements of a Residential Facade



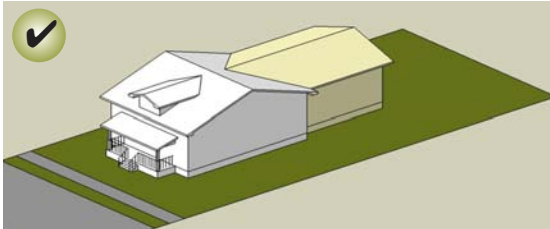
Guideline:

- Preserve the character-defining features of historic residential buildings. The typical features include the front porch, building form and materials.

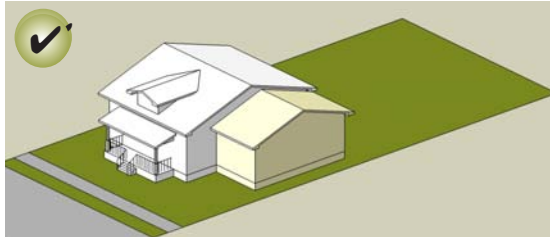
Designing A Residential Addition to a Historic Building



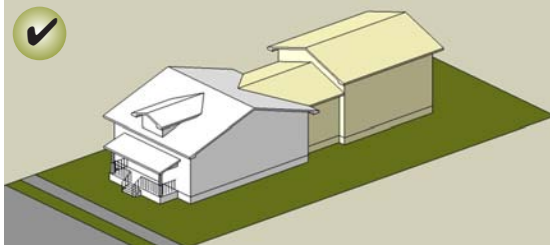
Original building
One-and-a-half stories



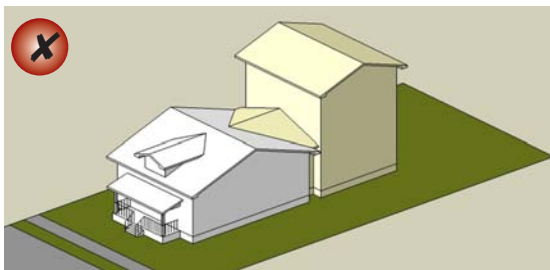
One story attached addition
Addition is set back behind the original building.



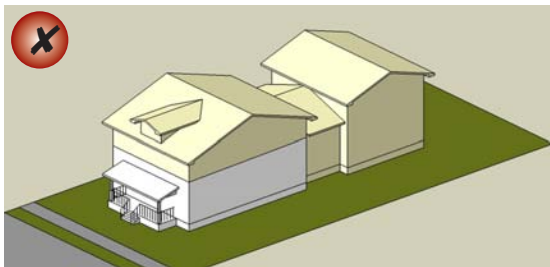
One story attached addition
Addition is set to the side of the original building.



One-and-a-half story addition with connector
Addition is set back behind the original and accessed by a connector.



Two-and-a-half story addition with connector
Addition is set back behind the original and accessed by a connector.



Two story roof-top addition
Addition is set back behind the original and accessed by a connector.



Incorporate traditional building components such as a storefront into the design of a new building (Boulder, CO).

5.3 New Construction

Things to See

Storefronts that create a browsing environment, with goods and activities to see in windows, invite walking.

Guidelines:

- Consider incorporating contemporary interpretations of traditional (1920's Commercial Style) designs and details into a new building. For example, new storefronts can create interest while expressing new compatible design.
- Incorporate traditional building components into the design of a new storefront or porch feature.



Incorporate traditional building components such as a base, middle and cap into the design of a new building (Boulder, CO).



Porches help connect buildings to the street and convey a sense of human scale (Georgetown, TX).



A new building should continue to use a variety of pedestrian-friendly scales and visually appealing masses (Missoula, MT).

A Variety of Building Types

Traditional commercial storefronts are essential ingredients of an urban village, as are conventional residential structures, including those adapted to commercial uses. Porches serve as “connections” to the street and as places for product displays. Storefronts offer views to goods and services inside. Building materials also vary.

Guidelines:

- Design a new building to reflect its time, while respecting key features of its context.

Human Scale Buildings

In the urban village, most buildings range in height from one to three stories, and they are designed to convey a sense of human scale.

Traditionally, commercial buildings had varied heights, articulated masses and visually interesting, pedestrian-scaled street fronts that contribute to a sense of human scale. A new building should continue this tradition of a variety of pedestrian-friendly scales and visually appealing masses. It should not be monolithic in scale or greatly contrast with those seen traditionally in the Village.

Guidelines:

- Maintain the traditional size of buildings as perceived at the street level.
- Establish a sense of human scale.
- Maintain traditional pattern of building widths along the street.
- A new building should incorporate a base, middle and cap.



Gaps in the building wall should incorporate small courtyards or seating areas with landscaping or street furniture.



Orient the primary entrance to the street (Milwaukee, WI).



Unlike a downtown where all buildings align uniformly at the sidewalk edge, an urban village will have more gaps (Le Claire, IA).

Building Entrances Oriented to Pedestrian Ways

Storefronts that face the street and invite walking are key features, as are shops that open onto alleys and internal paths. Residences with stoops and porches that signal connection to the street also contribute to the urban village character.

Guidelines:

- Maintain the traditional orientation of a building to the street.
- Enhance building entries oriented to the alley between East 11th and 12th streets. See “Village Lane” on page 36 for more information on alley improvements.

Variations in the Street Wall

Unlike a downtown where all buildings align uniformly at the sidewalk edge, an urban village will have more gaps. These spaces are designed to be used and should include plants and landscape structures.

Guidelines:

- Maintain the diverse building and open space pattern that occurs along the street.



High Quality in All Things

Finally, an urban village conveys a high level of quality, in terms of the goods and services provided, the buildings and infrastructure and in the experiences that villagers encounter.

Guidelines:

- Use high quality, durable building materials, such as masonry brick for commercial buildings and wood lap siding for smaller residential type buildings.
- Window and door openings as well as trims should be similar in profile to traditional openings.
- Synthetic material is generally inappropriate.



Use high quality, durable building materials, such as masonry brick for commercial buildings (Boulder, CO).

Design Guidelines for the Ball Diamond at Lindsay Park

Intent: To establish design-sensitive guidelines for signage at the ball diamond at Lindsay Park.

Background: The ball diamond at Lindsay Park was established well before 1900, perhaps as early as the days of Camp McClellan during the American Civil War. It is located within the Village of East Davenport Historic District.

The Davenport Parks and Recreation Department sells advertising space at the ball diamond to provide a source of revenue for the City. Advertising signs have long been accepted at ball parks throughout the country.



Ebbet's Field, Brooklyn, ca. 1940



Rickwood Field, Birmingham, Alabama, present day.

Advertising signs, if left unchecked, can be unsightly and detract from the overall look and feel of the East Village. Further, the City of Davenport, as its owner, desires to complement the ball diamond and the East Village in general by establishing design standards for commercial signage.

Design Standards:

- Each sign panel be sized to fit post to post, ground to the base of the yellow “safety” padding at the top of the existing fence (see Rickwood Field example).
- Advertising shall only be displayed towards the interior of the ball park; signage directed towards River Drive shall be prohibited.
- The back of the signs shall be colored Pantone Pine Green, 5923 TCX, or equivalent.
- The ‘batter’s eye’ shall also be protected with panels or other covering also in the same Pine Green color.
- Signage shall exemplify the “Golden Age of Baseball”, commonly identified as the period 1920-1960. Signs shall use design and fonts typical of the era. Generally speaking, signs shall use bold lettering, primary colors and dark, backgrounds. Actual “period” advertising from companies is encouraged. Bright neon colors are not appropriate.
- Signage shall be printed on 3mm Alu-panel (Aluminum/Plastic Combination).
- Damaged signs shall be repaired or removed within 30 days of incident.

Following are examples of appropriate signage:



Design Approval Process.

Before installation, the applicant shall obtain approval from a committee comprised by representatives of the Parks and Recreation and Community Planning and Economic Development Departments. Appeals may be made to the City Design Review Board.

City of Davenport

Department: Development & Neighborhood Services
Contact Info: Matthew Reu | 563-888-2221

Action / Date
3/23/2026

Subject:

Case DR26-01: Request for Design Review, C-E Elmore Zoning District; construct Stuff Etc. located at Jersey Farms Commercial Park 2nd Addition Lot 1. MGNS LLC petitioner. [Ward 8]

Recommendation:

Staff recommends the Board hold discussion and provide feedback for Case DR26-01 color, materials, and site design.

Background:

The Applicant is developing a new 165' x 165' retail building for Stuff Etc with new landscaping, parking lot, and signage in the southeast portion of C-E Elmore Corners Commercial Zoning District. C-E requires design review from the Design Review Board (DRB). DRB will provide design review for the development's site design, facades, and compatibility with the area's character – all in respect with the Elmore Corners Area Plan (The Plan). Signage will be discussed at a future meeting.

Elmore Corners Design Standards

C-E Elmore Corners Commercial Zoning District was adopted as ordinance in July 2018 by City Council. The district is intended to establish attractive patterns of development, be compatible with current development like a nearby single-family home subdivision and the casino, and capture I-80 traffic. C-E is situated between Jersey Ridge Road, I-80, I-74, and just West of Walmart on Elmore Avenue. Along with usual zoning regulations like dimensional standards, C-E is accompanied by the Elmore Corners Area Plan (The Plan).

The Plan encompasses design guidelines to achieve the desired development. The Plan divides Elmore Corners into six sub-areas: (1) Entertainment District, (2) Master Planned Development, (3) Mixed-Use Neighborhood, (4) North End, (5) Elmore Corridor, (6) Retail Infill. Each sub-area guides development differently, aiming to achieve a desired character. Elmore Corners Zoning District requires that all developments follow the Plan which establishes a preferred uniform building composition, facade, landscaping, setbacks, signage, circulation, parking, utility areas. C-E places the approvals with the Design Review Board (DRB).

Master Planned Mixed Use Development Area

The Master Planned Mixed Use Development Area is designed to facilitate large-scale development of two-to-three story buildings. The dense mixed-use area is to be anchored by a retail destination surrounded by pedestrian-orientated urban space. This sub-area encourages surface parking during the initial phases of development. Then, future development should utilize parking structures.

Sub-Area Criteria

1. Anchor Retail Destination

2. Cross connection between properties
3. Walkable Connections Between Uses
4. Pedestrian Oriented design
5. Shared Parking

C-E Criteria

- Area's Character
- Façade Design
- Site Design (landscaping, parking, circulation)

Staff Discussion

The Applicant's site plan shows a parking lot in the front of the lot, cross-site connection from the southern private access, dumpster in the rear, and landscaping. The Applicant proposes to construct pre-engineered metal building. The exterior façade is described as an architectural metal panel system, glazed storefront window, and accent material. The metal panel system is a Butler Manufacturing product name Shadowall™ Metal Wall Panel Systems, an insulated corrugated metal panel.

City Ordinance 17.05.070.A states that Elmore Corners Zoning District standards (C-E) purpose is to implement the vision contained in the Elmore Corners Area Plan (The Plan) through site and building design. 17.05.070.C states that the design requirements are described in the Elmore Corners Plan.

The Plan, in Chapter 5, page 26-45, details the Design Development Standards. The Plan divides design elements into "appropriate" and "inappropriate". Reframing from restating The Plan in this staff report, below is a short list of inappropriate design elements contained in the application Staff are concerned about:

- Plain, undersigned building with large blank spaces (page 27)
- Small buildings with single occupants
- Large unarticulated wall planes in three of four sides (page 28)
- Freestanding commercial structures shall be designed as four-sided architecture with Plan's desired finish grade materials (page 29)
- Heavy use of corrugated metal
 - The Plan calls for brick, stone, masonry, glass for natural earth tone colors
 - Secondary materials should be metal, wood, stucco, cement board
- Buildings set back over 40 feet from the right-of-way (page 30)
 - Setback 0-10 feet along secondary roads
 - Screen parking with landscaping
- Parking lot between the right-of-way and the primary façade (page 30, 34, 35)
 - Minimize space between buildings
- Individual parking lots for each business
- Numerous access drives into disconnected parking areas
- Parking lots overbuilt and underused (page 35)
- Parking lots without pedestrian walkways (page 36)

Appropriate design elements present:

- Defined base, middle, top elements in front façade (page 26)
- Parallel to right-of-way (page 27)
- Service area (trash) in rear and screened (28, 38)
- Advanced stormwater management practices
- Cross-access provided to future lots in the South

Past C-E Plan Application: Aqua-Tech

In short, the sub-area was envisioned to be a new-urbanist mixed-use shopping center in the City of Davenport. The applicant proposes a single story, single-use, auto-oriented development. The proposal is not consistent with the urban pedestrian-oriented goal of C-E and should be denied. However, Aqua-Tech Car Wash at 6280 Elmore Ave, Case DR20-02, was previously approved in C-E by the Design Review Board on January 27th, 2020.

Then, staff stated similar language contained herein, that the proposed development was not aligned with the design intention of C-E. The Applicant then encouraged DRB to consider that the project is a stand-alone project, like sub-area called Retail Infill. DRB concluded that they recognized the project as part of the Mixed-Use Neighborhood sub-area. DRB required that the Application build a second story to meet The Plan's guidelines. The site was permitted to not comply with the cross-connection requirement of The Plan.

DRB also reviewed the façade materials. Aqua-Tech originally planned to use Nucor Panel – Steel Wall Panels (corrugated metal) and Stone Vainscot – a polyurethane mimic of stone. DRB and the Zoning Board of Adjustment permitted the construction of Aqua-Tech (PRJ-1629) and required they utilize materials that align more with The Plan. The following façade materials were used:

First Floor Façade:

- Nuform Panel | PVC | Concrete Wall Panel (primary façade material)
- Aluminum Storefront Glazing
- Limestone (at foundation)

Second Floor Façade

- Nichiha | Fiber Cement Siding (primary façade material)
- Fiberglass Windows
- Aluminum Storefront Glazing

Sub-Area: Retail Infill

Staff encourage DRB to discuss a small part of The Plan called Retail Infill. Retail Infill is a sub-area of the Plan to permit standalone sites for retail, services, restaurants, or other commercial spaces along Elmore Ave. Retail infill encourages parking in the rear, building face Elmore Ave, and locate the building near Elmore Ave. Buildings in Retail Infill are anticipated to be one to two stories. It does not require cross-connection. Projects in Retail Infill are meant to stand on

their own. Staff believe that Retail Infill, originally intended to be West of Wal-Mart which declined to be a part of C-E, does not have a defined location, and debate on the sub-area bubbles as hard set locations. Staff ask DRB to consider the possibility of permitting limited Retail Infill.

Sub-Area criteria:

- 1-2 Story commercial
- Parking encouraged in the rear
- Primary facade facing Elmore Avenue
- Cross connection encouraged

Applicant Comments

Staff have summarized with the intention to best represent the statements of the Applicant. Full narrative provided by the development team is attached to the staff report. Additional comments are included in the presentation.

Elmore Corners Plan establishes a vision for high-quality development supporting economic activity, retail corridor, and encourage thoughtful design. The proposed project for Stuff Etc. represents an opportunity to introduce new investment while creating meaningful momentum for Elmore Corners. The proposed development should be considered as a Retail Infill in accordance with the Elmore Corners Area Plan. This development represents a logical extension of the southern retail center, where Wal-Mart is now. Stuff Etc. will activate the site with visible commercial activity, encouraging more development along Elmore Ave and supporting the Elmore Corners Area Plan.

The development has been mindful for cross-connection opportunities. Future development can connect via the private drive to the south of the proposed development. Additionally, future development could connect from the North. Northern development will connect in the rear of the proposed development.

The proposed building is a 27,000 square foot pre-engineered metal building, a common construction method in modern commercial development due to efficiency, durability, and adaptability. These elements work together to create a visually engaging façade while maintaining the construction efficiency necessary to support viable commercial development. The exterior incorporates a variety of architectural enhancements including:

- Architectural metal panel systems
- Storefront glazing systems along the public-facing façade
- Accent materials and detailing to provide visual articulation
- Insulated metal panels integrated into the overall façade design

The design team has been mindful of the requirement to provide approximately 40 percent façade transparency between two and ten feet above grade along the public-facing elevation. Exterior elevations and dimensional information will be provided to demonstrate compliance. Staff and the Development Team will continue to work together to ensure compliance.

The site has been carefully considered to create a visible, vibrant commercial presence along Elmore Avenue while responding to the unique topography and physical constraints of the site. A natural berm located along the northern portion of the property limits visibility from Elmore Avenue from that direction and presents challenges for development orientation. As a result, the building has been positioned to maximize its visibility and engagement with the Elmore Avenue frontage. The building placement also helps establish a more logical and functional development pattern for surrounding parcels. The configuration of the site helps “square off” the more challenging portions of the property, which have historically limited the ability to implement cohesive development. By defining the primary development edge along Elmore Avenue and organizing circulation around the building, the project creates a more practical framework for future investment on adjacent land. In this way, the project not only activates the site today but also helps organize the area for more successful development in the future.

The Staff organization and the development team argue that this development has the potential to serve as a catalyst for additional investment in this portion of the Elmore corridor. While the southern portion of the corridor has seen strong retail growth, this area has experienced more limited development activity despite earlier infrastructure investment by the city. Introducing an attractive and active commercial building at this location can help stimulate new interest in surrounding properties and encourage additional development over time.

The project represents an estimated \$4 million private investment in the Elmore corridor and is considered a shovel-ready project, with construction anticipated to begin shortly after approvals are secured. The project is best understood as retail infill that helps organize a challenging piece of land while establishing a framework for future development around it. By introducing visible commercial activity along Elmore Avenue, the project strengthens the corridor, creates momentum for additional investment, and supports the long-term vision for coordinated development in the Elmore Corners district. Our goal here is simple — create visible commercial energy on Elmore and organize this piece of land so future development becomes easier, not harder.

Recommendation

Staff recommend that DRB discuss and provide direction and feedback to the Applicant. Today, DRB should decide what sub-area the development falls under and site design considerations.

DRB can approve, approve with conditions, table, or deny the project. If DRB approves or approves with conditions, the Applicant has one year from date of the meeting to start the project. The Applicant may apply in writing for an extension or submit another DRB application. If DRB wishes to table the application to continue the discussion or if DRB feels there is not enough information, they can table the application a maximum of three times unless the Applicant requests additional meetings. If DRB tables the item, DRB should specifically request more information, provide feedback, or something actionable for the Applicant to bring back to DRB. Should DRB deny the application, the applicant can appeal the decision to City Council. A written appeal must be submitted to the Zoning Administrator within thirty (30) days of DRB’s decision.

Approve / Approve with Conditions

- one year from date of the meeting to start the project

Applicant may apply in writing for an extension or apply for submit another DRB application Table, or

- Can table if more information is desired or require more conversation
- Can table 3 times,
 - Else, Applicant can request additional meetings

Deny

- Applicant can appeal the decision to City Council by mailing letter of appeal to the Zoning Administrator in 30 days of DRB's decision.

Attachments:

1. Developer Narrative
2. Elmore Corners Area Plan 11-03-17
3. Application

Development Narrative Draft Humes / Naumann/Eikenberry – March 7, 2026 (Rev. 3)

Proposed Commercial Development Elmore Corners District – Davenport, Iowa

Prepared by **Milestone Construction Group & Hawkeye Commercial**

Milestone Construction Group is pleased to present this proposed commercial development within the **Elmore Corners District**, an area identified in the City of Davenport’s 2017-18 Elmore Corners Master Plan as a key opportunity for coordinated growth along the Elmore Avenue corridor.

The Elmore Corners Plan established a vision for high-quality commercial development that supports economic activity, strengthens the retail corridor, and encourages thoughtful site planning as the area evolves. The proposed project aligns with these objectives and represents an opportunity to introduce new investment while creating meaningful momentum for this important commercial corridor.

Retail Infill Opportunity

The proposed development should be considered **retail infill within the existing Elmore Avenue commercial corridor**.

The site sits immediately north of a well-established cluster of major retail destinations and commercial activity along Elmore Avenue. As development has progressed southward along the corridor, this property represents a logical extension of that retail environment.

The project introduces a vibrant retail business that complements surrounding businesses and contributes to the strength of the corridor. By activating this site with visible commercial activity, the development strengthens the continuity of the Elmore Avenue retail corridor and supports the broader vision outlined in the Elmore Corners Plan.

The development team has also been mindful to maintain opportunities for **future cross-access and connectivity** with the parcel located immediately to the south. As additional development occurs in this area, the site layout will allow circulation and development patterns to integrate naturally between properties, supporting the long-term vision of coordinated development along the corridor.

Site Configuration and Building Placement

The placement of the building has been carefully considered to create a **visible, vibrant commercial presence along Elmore Avenue** while responding to the unique topography and physical constraints of the site.

A natural berm located along the northern portion of the property limits visibility from Elmore Avenue from that direction and presents challenges for development orientation. As a result, the building has been positioned to maximize its visibility and engagement with the Elmore Avenue frontage, where the majority of corridor traffic and commercial activity occurs.



This placement ensures that the business activity occurring within the building remains highly visible to passing motorists and visitors traveling along Elmore Avenue, reinforcing the corridor's identity as an active commercial destination. Locating the primary commercial activity closer to the roadway allows the energy and vibrancy of the business to be visible from the corridor rather than hidden toward the rear of the property.

The south side of the property will feature attractive landscaping designed to soften the site and create a welcoming transition to adjacent parcels. While this portion of the site will experience less regular customer activity, the landscaping and site design will maintain a clean and visually appealing edge for the property.

The Stuff organization also plans to incorporate a positive and creative design theme within the facility. While the south wall will have less visibility from the primary corridor, the owners remain committed to ensuring the building presents a clean and attractive

appearance from all directions. The facility is intended to be a development Davenport can be proud of today and for years to come.

Establishing a Strong Framework for Future Development

Beyond serving as a stand-alone commercial project, the building placement also helps establish a **more logical and functional development pattern for surrounding parcels.**

The configuration of the site helps “square off” the more challenging portions of the property, which have historically limited the ability to implement cohesive development. By defining the primary development edge along Elmore Avenue and organizing circulation around the building, the project creates a more practical framework for future investment on adjacent land.

This approach creates stronger opportunities for **future commercial or multi-family development to the north**, allowing those properties to integrate into the corridor with more functional site layouts and improved access.

In this way, the project not only activates the site today but also helps organize the area for more successful development in the future.

Market Conditions and Development Feasibility

While the Elmore Corners Master Plan established an important long-term vision for development along the corridor, economic conditions have changed significantly since the plan was adopted in 2018.

Commercial construction costs have increased substantially in recent years due to inflation, rising labor costs, supply chain disruptions, and significant increases in the price of construction materials. These changes have made it considerably more difficult to implement development concepts that may have been financially feasible when the original vision for the corridor was developed.

These challenges are amplified on sites with physical constraints such as irregular lot configuration, limited cross-connectivity, and reduced visibility caused by topography. In

these situations, development approaches must carefully balance design goals with economic feasibility to ensure projects can realistically move forward.

The proposed development represents a practical and responsible response to these market realities. By utilizing an efficient pre-engineered building system and positioning the building to maximize visibility along Elmore Avenue, the project creates a development that is both economically viable and supportive of the broader goals of the Elmore Corners Plan.

Rather than leaving the property undeveloped while waiting for market conditions to align with earlier conceptual layouts, this proposal provides an opportunity to introduce real commercial activity, generate private investment, and help build momentum for additional development within the corridor.

In this way, the project reflects a pragmatic evolution of the plan's vision—one that recognizes current economic realities while still advancing the long-term goal of strengthening the Elmore Avenue commercial district.

Architectural Design and Building Materials

The proposed building will utilize a **pre-engineered metal building system**, a construction method widely used in modern commercial development due to its efficiency, durability, and adaptability for future tenants.

While the structural system utilizes pre-engineered construction, the exterior design incorporates a variety of architectural enhancements intended to create an attractive and contemporary commercial building along the corridor.

Exterior materials will include:

- Architectural metal panel systems
- Storefront glazing systems along the public-facing façade
- Accent materials and detailing to provide visual articulation
- Insulated metal panels integrated into the overall façade design

These elements work together to create a visually engaging façade while maintaining the construction efficiency necessary to support viable commercial development.

Transparency and Streetscape Engagement

The building façade facing Elmore Avenue will incorporate storefront glazing and transparent elements designed to engage the public realm and meet the City’s design guidelines.

The design team has been mindful of the requirement to provide **approximately 40 percent façade transparency between two and ten feet above grade** along the public-facing elevation. Exterior elevations and dimensional information will be provided to demonstrate compliance with this requirement and to ensure the building contributes to an active and visually appealing streetscape.

Catalyst for Corridor Investment

In addition to meeting the immediate needs of the project, the development has the potential to serve as a **catalyst for additional investment in this portion of the Elmore corridor**.

While the southern portion of the corridor has seen strong retail growth, this area has experienced more limited development activity despite earlier infrastructure investment by the city. Introducing an attractive and active commercial building at this location can help stimulate new interest in surrounding properties and encourage additional development over time.

By creating visible commercial activity along Elmore Avenue, this project can help generate momentum that supports future investment, strengthens the corridor’s identity, and helps the community realize greater value from infrastructure improvements made in previous years.

Project Details

The proposed development consists of a **27,000 square foot commercial building** supported by a parking and circulation layout designed to meet municipal standards.

The project represents an estimated **\$4 million private investment** in the Elmore corridor and is considered a **shovel-ready project**, with construction anticipated to begin shortly after approvals are secured.

The development team anticipates the facility could be completed and open for business by **December 2026**.

Lighting for the site will meet all municipal standards and will include decorative and customized lighting elements within the parking lot and on the building. A photometric study will accompany the development plans to ensure lighting levels remain appropriate at the property line so that neighboring properties and passersby are not adversely impacted.

Site Availability and Development Opportunity

The Stuff ownership group has been actively exploring opportunities to locate within the Elmore Avenue retail corridor for some time. During that process, several potential sites within the corridor were evaluated, including locations closer to the existing Walmart out-lot area.

However, many of those properties are currently controlled by large institutional landholders who have indicated a preference for long-term ground leases or have not been actively marketing their properties for development. As a result, opportunities for direct purchase and immediate commercial investment in the corridor have been limited.

The subject property represents a unique opportunity where the ownership structure and purchase terms have aligned to allow a commercial investment to move forward. The site is currently under contract for purchase at mutually agreeable terms, subject to the review and approval process with the City of Davenport.

Because the development team has secured a site where a project can realistically move forward, this proposal represents a near-term opportunity to introduce new commercial

activity within the corridor, helping generate the momentum that has been envisioned for this portion of Elmore Avenue.

In this way, the project not only fulfills the goals of the Elmore Corners Plan but also demonstrates how private investment can begin activating underutilized areas of the corridor when viable sites become available.

Commitment to Quality Development

Milestone Construction Group approaches projects through its **Design – Build – Deliver** philosophy, emphasizing collaboration, efficiency, and quality outcomes.

Through this integrated process, the development team works closely with designers, city staff, and project partners to deliver projects that respond to market needs while supporting the planning vision of the community.

Milestone Construction Group looks forward to working with the City of Davenport and the Design Review Board as this project advances and contributes to the continued evolution of the Elmore Corners district.

Conclusion

As the development team evaluates the site and the broader corridor context, the project is best understood as **retail infill** that helps organize a challenging piece of land while establishing a framework for future development around it.

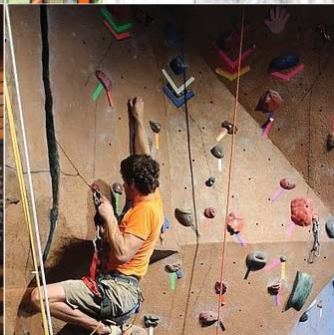
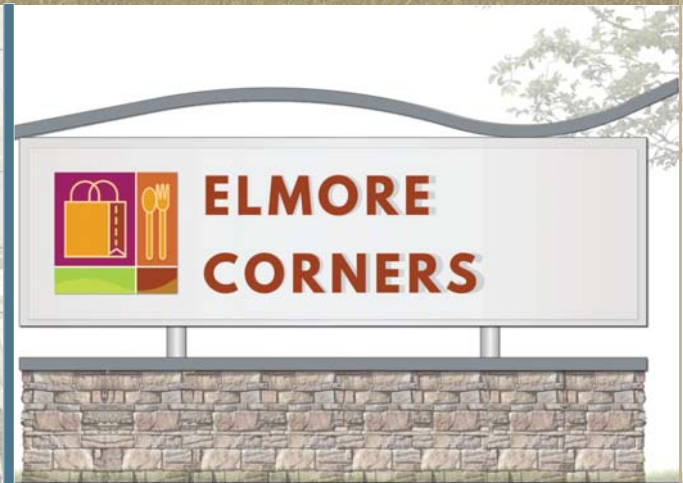
By introducing visible commercial activity along Elmore Avenue, the project strengthens the corridor, creates momentum for additional investment, and supports the long-term vision for coordinated development in the Elmore Corners district.

Our goal here is simple — create visible commercial energy on Elmore and organize this piece of land so future development becomes easier, not harder.



Elmore Corners Plan

Davenport, Iowa



November 3, 2017

Planning assistance by:

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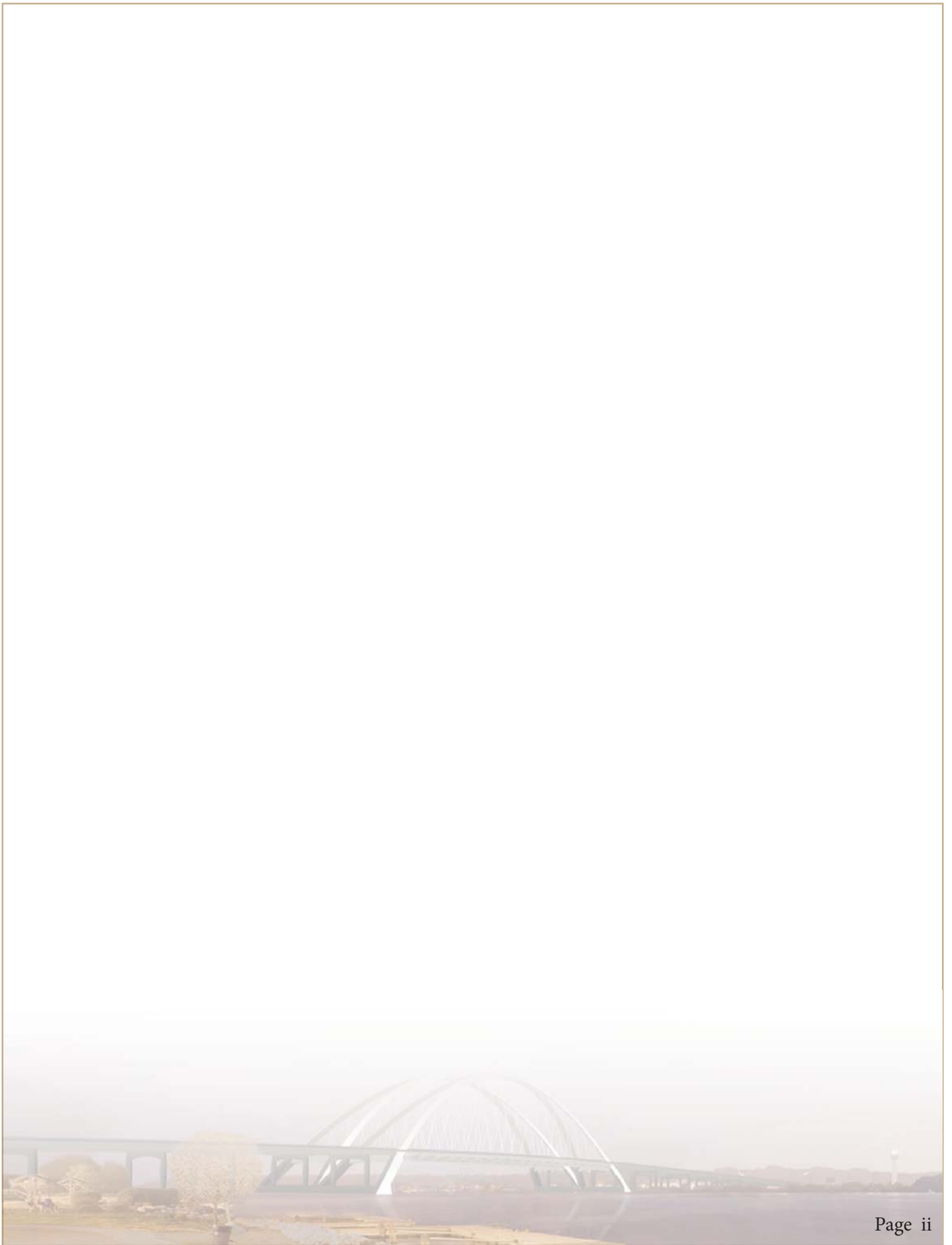
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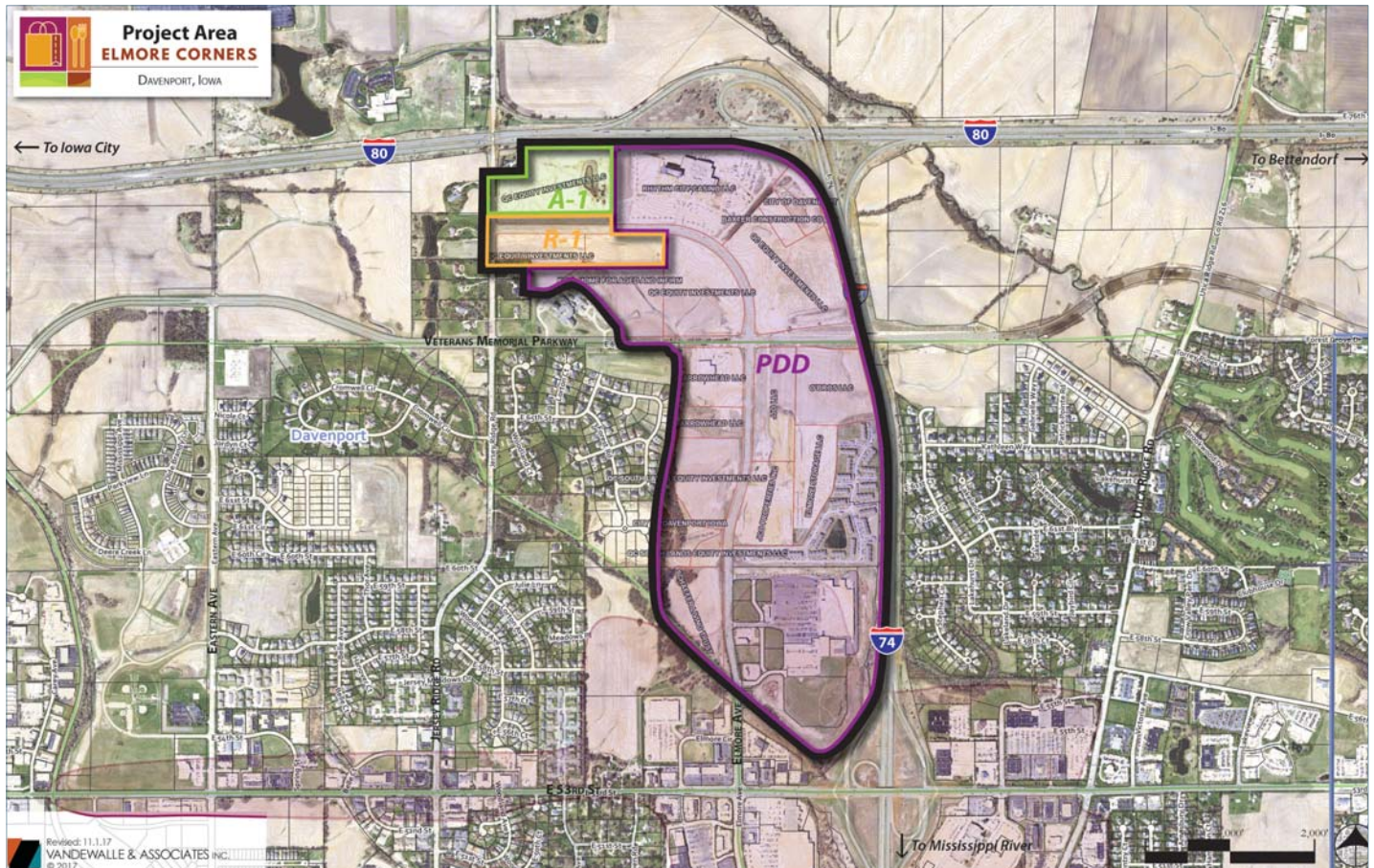
I. Introduction

PROJECT AREA

Located on the northeast side of Davenport, Iowa, at the intersection of Interstate 80 and Interstate 74, the approximately 264-acre project area is sited at a strategic community location. The project area serves as a community gateway from the Interstate 80 corridor. Generally, surrounding land uses include Interstate 80 and agricultural land to the north, single-family homes to the west, retail and commercial to the south, and Interstate 74 and single-family homes to the east.

As shown on Map 1, the majority of the planning area is currently zoned Planned Development District (PDD) with a few R-1 and A-1 parcels, and eight property owners control the majority of the properties. The entire project area falls within the Highway Corridor Overlay Zoning District. Current uses in the project area include a casino, automobile sales lot, apartments, retail, warehouse storage, senior living, and agriculture.

Map 1. Project Area



PROCESS

The planning process took place from July to October of 2017 and the City of Davenport Staff served as the project steering committee. Staff provided guidance throughout the planning process and approved all plans, drawings and recommendations. The steering committee reviewed draft plans two times throughout the project before deliverables were finalized.

Project Kick Off Meeting and Stakeholder Interviews – July 18, 2017

Vandewalle & Associates traveled to Davenport to meet with City Staff, City Officials and project stakeholders. During these meetings, stakeholders provided initial project input and suggestions. Property owners provided plans for their properties and future land uses and improvements were brainstormed. Additional stakeholder input was provided via teleconferences throughout the planning process.

Draft Presentation – October 25, 2017

Vandewalle & Associates presented the draft Site Layout Plan, Pedestrian Circulation and Streetscape Concept, Public Street Network, Landscape Concept, Wayfinding Concept, Branding Concept, and Design Development Standards via a web conference. The steering committee provided feedback and asked questions during the meeting. After the presentation, Vandewalle & Associates finalized all drawings and summarized them into this document, *Elmore Corners Plan*.



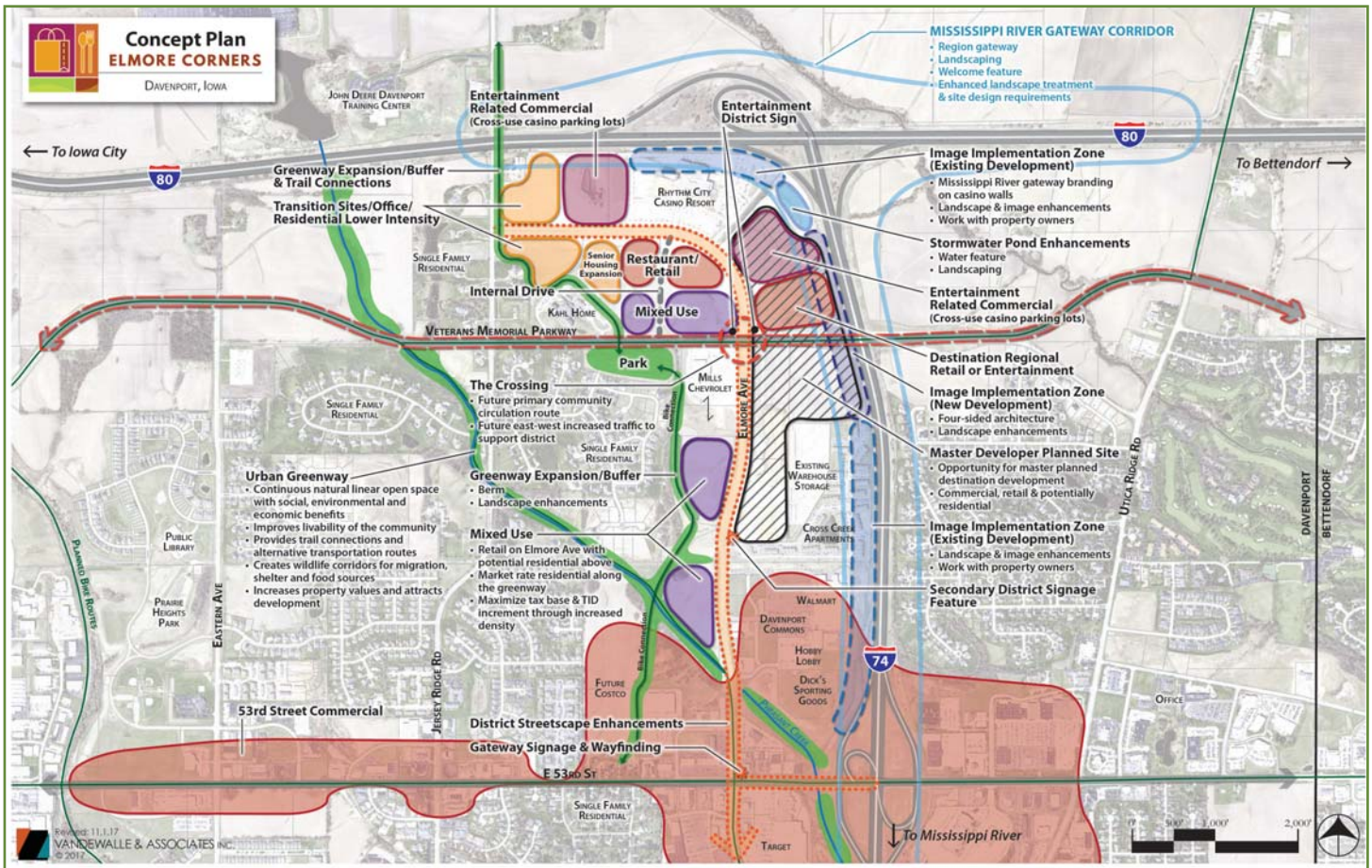
Elmore Corridor



II. Concept Plan

As seen in Map 2, the Concept Plan establishes the planning and development vision for Elmore Corners in the context of adjacent neighborhoods and existing land uses. The map highlights proposed future land uses and public infrastructure. The illustration also identifies important design issues to be considered for a successful development. These are explained further below and in the Design Development Standards section.

Map 2. Concept Plan



POTENTIAL FUTURE DEVELOPMENT

Future development should be a sustainable mix of land uses that are market feasible, serve current community needs, maximize tax base, and support the success of the Rhythm City Casino. Visual access from two interstate highways and traffic generation from the casino create a strong market for retail. While the current retail market is in transition from “brick and mortar” to “on-line” shopping, future development around the casino should be tailored toward experiences and entertainment. Uses that complement the casino should be encouraged in the northern planning area.

An opportunity exists to create a mixed-use neighborhood through a master planned site located between Interstate 74 and Elmore Avenue. This area is sufficient in size to develop retail, commercial, office and residential development along a secondary street system. Lands between the senior housing project and Elmore Avenue should be a mix of commercial related to the casino and residential uses. This large block should be broken up with additional public streets to provide access into the site. Land uses along Jersey Ridge Road should decrease in scale, massing, and vehicular trip generation to create a transition in development intensity between the casino entertainment area and existing single-family neighborhoods to the west.

Along Elmore Avenue, north of the Pheasant Creek corridor, provides a natural break in land use and an opportunity to transition from strictly retail/commercial uses located along 53rd Street to a more integrated neighborhood. These sites should contain mixed-use buildings along Elmore Avenue and around the intersection of Elmore Avenue and Veterans Memorial Parkway. First floor commercial uses will maintain the Elmore Avenue commercial street edge while residential or office above increases development density and tax base. The area between these mixed-use buildings and the proposed greenway is suitable for multi-family residential development. Density could be increased since the residents will have access to the greenway for alternative transportation to public streets.

Potential land uses are further explained in the following Site Layout Plan section of this document.

THE CROSSING

The intersection of Elmore Avenue and Veterans Memorial Parkway will be a major community intersection in the near future. With the completion of Veterans Memorial Parkway construction, a new east/west corridor will run through Davenport, providing vehicles with a new transportation route. These automobile trips will create a commercial market at this intersection and along the parkway. The intersection also becomes an important wayfinding point, identifying the Elmore Corners Entertainment District to the north and retail and commercial to the south.



Example Potential Future Development Uses



IMAGE IMPLEMENTATION ZONES

These areas along the interstate are highly visible and are often the first image of Davenport for visitors. The quality of development along the corridor is important to best represent the City. For this reason, development within the area should be of high quality architecture and site design. Buildings should have four-sided architecture, meaning quality design and materials on all sides of the building. Sites should contain aesthetically pleasing landscaping and storm-water management areas. Parking lots should be screened from the interstate and stormwater detention ponds should be naturalized in shape and landscaping.

MISSISSIPPI RIVER GATEWAY CORRIDOR

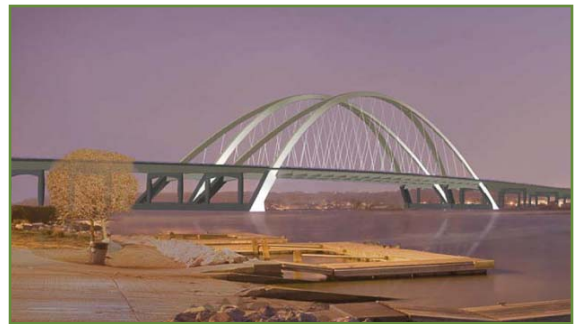
The Image Implementation Zones fall within a larger initiative to unify the image of the Interstate 74 corridor under the Q2030 Regional Plan, specifically the I-74 Gateway Corridor, part of the Prosperous Economy theme/pillar. Projects that fall within this area should highlight the community gateway through both high-quality architecture and community gateway signage. The region may work with the casino operators to use the blank casino walls to highlight region heritage, the Mississippi River, community events, etc. The space should be a lively welcome to visitors.

URBAN GREENWAY

A greenway system would provide numerous benefits to future residents and visitors of Elmore Corners. A greenway is a continuous and linear natural open space with environmental, social and economic roles. The natural role is to provide animals with a natural habitat for nesting, migration and food sources. Greenways can also be used for storm water infiltration and conveyance. The social role is to improve urban recreation opportunities, encourage community interaction, and provide an alternative transportation mode. Greenways also have economic benefits by increasing land values. Businesses located near greenways highlight this feature as an employee amenity and greenways aid in workforce retention and attraction. The greenway is proposed as a 100-foot wide corridor which will provide a buffer space between existing neighborhoods and new development. The public space can also be used to organize development by providing an off-street path accessible from new buildings.



Example Image Implementation Zone



Mississippi Gateway Corridor



Urban Greenway



NEIGHBORHOOD CONTEXT

Established single-family neighborhoods border the Elmore Corners planning area to the west. As development occurs, design attention must be paid to building placement, massing, design, setbacks, and lighting to ensure future development does not negatively impact these neighborhoods. The proposed Urban Greenway along the western project boundary will provide a natural buffer between established and proposed neighborhoods.



Neighborhood Context

STREETSCAPE AND WAYFINDING

The district public space image and signage will be as important as architecture in establishing a high-quality development. Public streets should contain decorative street lights, banners, and plantings. A consistent district wayfinding system should be installed to direct visitors to destinations and activities.



Example Wayfinding Signage

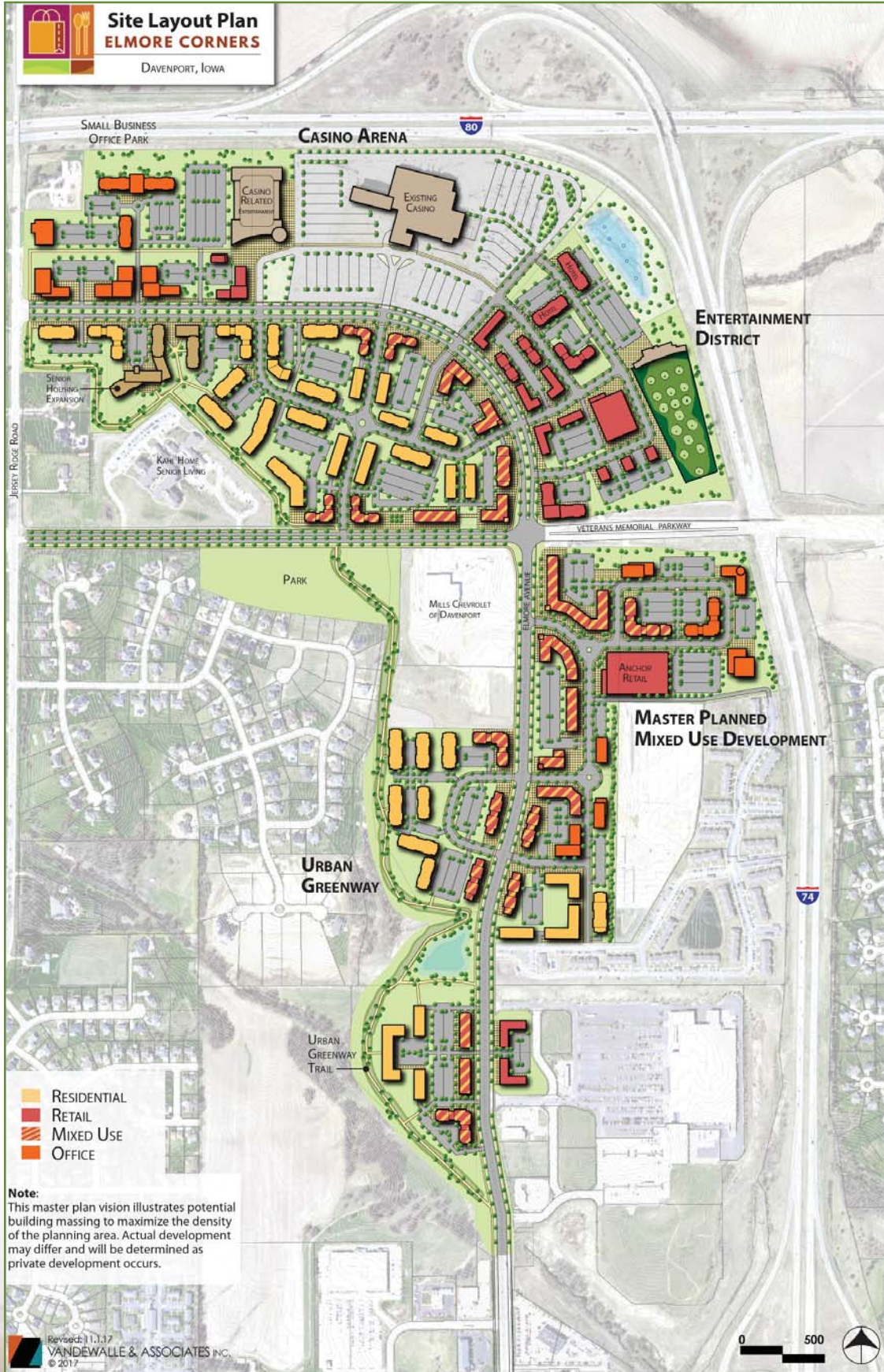


III. Site Layout Plan

The Site Layout Plan provides a vision for how Elmore Corners could develop to provide a community gateway development, maximize tax base, and create walkable and sustainable neighborhoods. The plan illustrates potential building massing, locations, and density, though actual development proposed by private developers will vary from this vision. The plan represents the steering committee desired development pattern.

In addition to future building locations, the Site Layout Plan also contains suggested marketable future land uses. This information is provided by project sub areas for ease of describing potential future development. The subareas are not intended to be branding names. These subareas are described in the following text and summarized on Map 4. Sub Areas on page 19.





ELMORE CORNERS ENTERTAINMENT DISTRICT

Being adjacent to the Rhythm City Casino and highly visible from the junction of two interstate highways, this area has the market potential to become a destination entertainment district. The area is envisioned as an active district providing visitors with numerous activities in a walkable environment. Future streets and walkways connect new development to the casino entrance. Future uses could be a mix of entertainment anchor uses for people of all ages. Parking should be shared between uses to maximize developable area and minimize parking lots and distances between uses. Buildings near the casino may be able to share parking with the casino.

Potential uses could include:

- Hospitality
- Regional retail
- Sports arena
- Sports training facilities
- Sports entertainment
- Indoor activity center of children and/or adults
- Music venue
- Restaurants
- Brew pub/micro brewery
- Health and wellness facility
- Water park resort

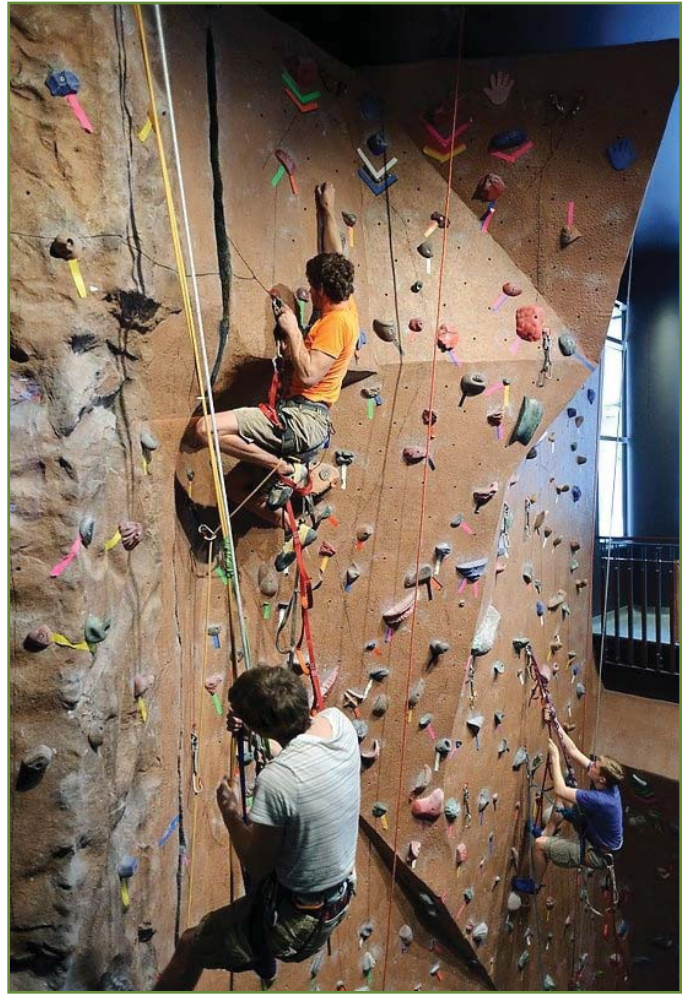
Quality four-sided architectural standards and enhanced landscaping will be especially important within this area, since it is highly visible from the interstate highways and falls within the I-74 Gateway Corridor, as identified in the Q2030 Regional Plan. Buildings in this area are anticipated to be one to five stories in height. The site layout plan shows the following approximate square footage at one story for retail and five stories for hospitality:

Retail/Commercial – 140,000 sf

Entertainment Anchors – 70,000 sf

Hospitality – 300 rooms





MASTER PLANNED DEVELOPMENT

This area provides an opportunity for a large-scale master planned development. With three property owners, the land may be developed jointly into a dense, mixed-use neighborhood. Anchored by a destination retail use, the neighborhood is envisioned to be urban in design with buildings located along new public streets providing pedestrians with an enjoyable and walkable connection between uses. First floor buildings should be commercial with office and residential on above floors. An area along Interstate 74 should be reserved for office sites providing employers with great highway visibility.

Potential uses may include:

- Mixed-use buildings with retail or services on ground floors and residential or offices above
- Retail - destination branded, i.e. "Shops at Elmore Corners"
- Restaurants
- Anchor retail – potential grocery store
- Office sites along Interstate 74 and Veteran's Memorial Parkway
- Structured parking to achieve critical density to create a walkable district

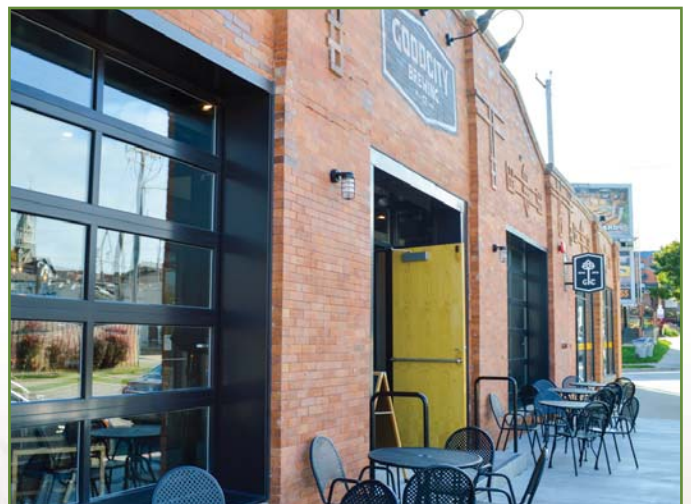
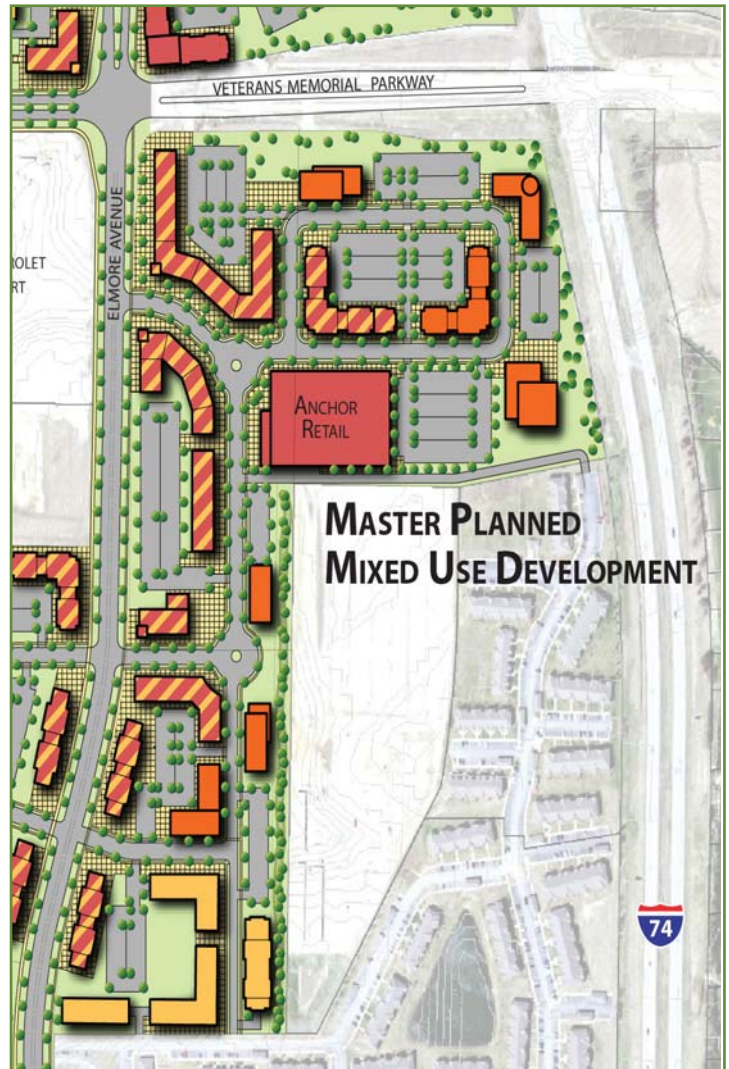
Surface parking lots could be initially constructed and be phased into structured parking as the site densifies. Buildings in this area are anticipated to be one to four stories in height. The site layout plan shows the following approximate square footage/residential units at three stories for office and mixed-use buildings and one story for the anchor retail:

Retail/Commercial – 290,000 sf

Anchor Retail – 95,000 sf

Office – 350,000 sf

Residential – 450 units





ELMORE CORNERS MIXED-USE NEIGHBORHOOD

This area, located between the Rhythm City Casino and senior housing along Veterans Memorial Parkway, should be developed into a residential neighborhood with commercial along the major streets. A future public street connecting the casino entrance to Veterans Memorial Parkway extends the commercial market from the Elmore Avenue/Veterans Memorial Parkway intersection west to the two new intersections. Mixed-use buildings are suggested along the primary streets to serve both district visitors, neighborhood residents, and citizens commuting along Veterans Memorial Parkway. These buildings provide excellent ground floor spaces for local retailers and service providers.

Residential is envisioned as the primary land use west of the casino and proposed north/south street. Future development could be a combination of multi-family housing and an expansion of the senior housing development. All buildings should have trail access to the greenway. The proposed north/south public street also provides a controlled crossing of Veterans Memorial Parkway, connecting the greenway across the street. Buildings in this area are anticipated to be three to four stories in height. The site layout plan shows the following approximate square footage/residential units at three stories:

Retail/Commercial – 140,000 sf

Senior Housing – 150,000 to 200,000 sf

Residential – 1,100 units





NORTH END

This area, located between Jersey Ridge Road and Rhythm City Casino, has excellent visibility from Interstate 80. While retail may not be suitable due to distance away from the intersection of Elmore Avenue and Veterans Memorial Parkway, the area may be attractive for companies to locate an office or headquarters. The master plan shows a large office building along the interstate with smaller office sites to the south, along Elmore Avenue. A small business office park could be established providing employees with access to retail and commercial, as well as, nearby housing. Users are anticipated to be in the 10,000 to 40,000 sf range, providing owner occupied opportunities.

The eastern edge of the area may be developed with a casino-related destination use such as an entertainment facility, arena, or indoor/outdoor water park which would generate traffic and provide market for support retail or restaurants. A destination use could utilize existing parking lots at the casino to reduce the need for additional paved area. A pedestrian connection should be constructed between this area, the casino, and the Elmore Corners Entertainment District. The connection could also extend to the west connecting office and commercial uses to the greenway.

Buildings in this area are anticipated to be one to four stories in height, though a corporate user along the interstate may be taller. Office buildings on the west end should be one to two stories to transition to the existing single-family neighborhood to the west. The site layout plan shows the following approximate square footage at three stories for office use and one story for retail and casino-related destination uses:

Retail/Commercial – 25,000 sf

Office – 420,000 sf

Casino-Related Destination – 60,000 to 120,000 sf





ELMORE CORRIDOR

Uses along Elmore Avenue are currently retail and commercial. As Elmore Corners develops into a sustainable neighborhood with a mix of uses, this area should be reserved for mixed-use buildings along Elmore Avenue and multi-family residential to the west. Mixed-use buildings may contain retail, commercial, or service uses on the ground floors with parking to the rear of buildings. Above floors may be residential or office. All buildings have walkway connections to both sidewalks along Elmore Avenue and the pathway in the proposed Urban Greenway. The latter offers an alternative transportation route, exercise area, and access to the natural environment.

This plan recommends removing the berm, located along the western edge of Elmore Avenue, across from Davenport Commons commercial development. This will allow future buildings along the street to have commercial ground floor uses. If the berm remains, then the uses on the site would be residential as the berm would block views of commercial properties. Buildings in this area are anticipated to be three to four stories in height. The site layout plan shows the following approximate square footage/residential units at three stories:

Retail/Commercial – 100,000 sf

Residential – 600 units



RETAIL INFILL

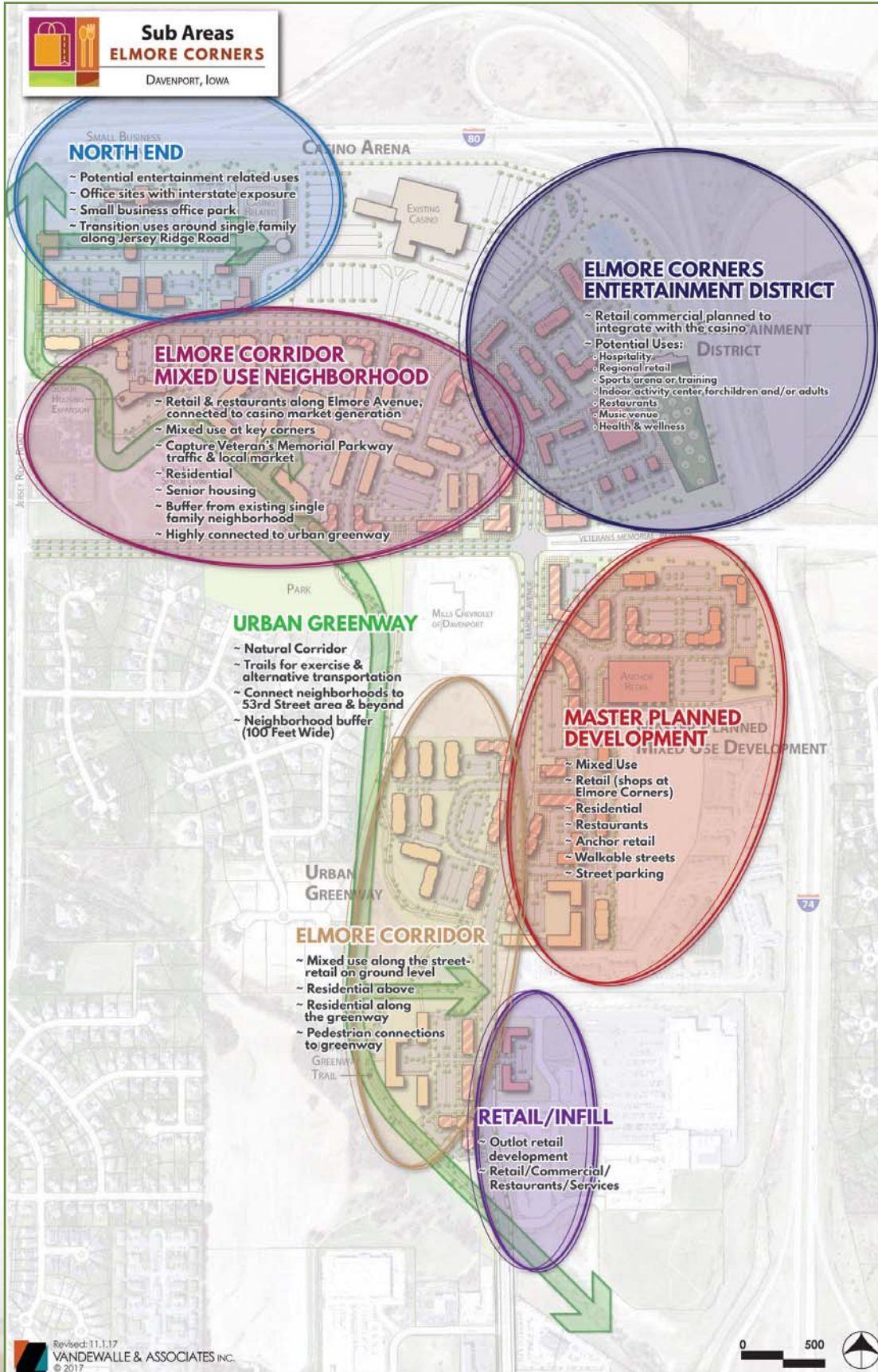
The final area of the Elmore Corners Plan is the out-lot sites along Elmore Avenue within Davenport Commons. These sites will most likely remain as sites for retail, commercial, service, or restaurants. Parking is encouraged to the rear to maintain a building face streetscape along Elmore Avenue. Locating the buildings near the public street will also announce Davenport Commons for visitors.

Buildings in this area are anticipated to be one to two story. The site layout plan shows the following approximate square footage at one story:

Retail/Commercial – 26,000 sf

The development areas and projects identified within the Site Layout Plan are not dependent upon each other to develop. Each project can stand on its own and could be a separate developer and/or phase.

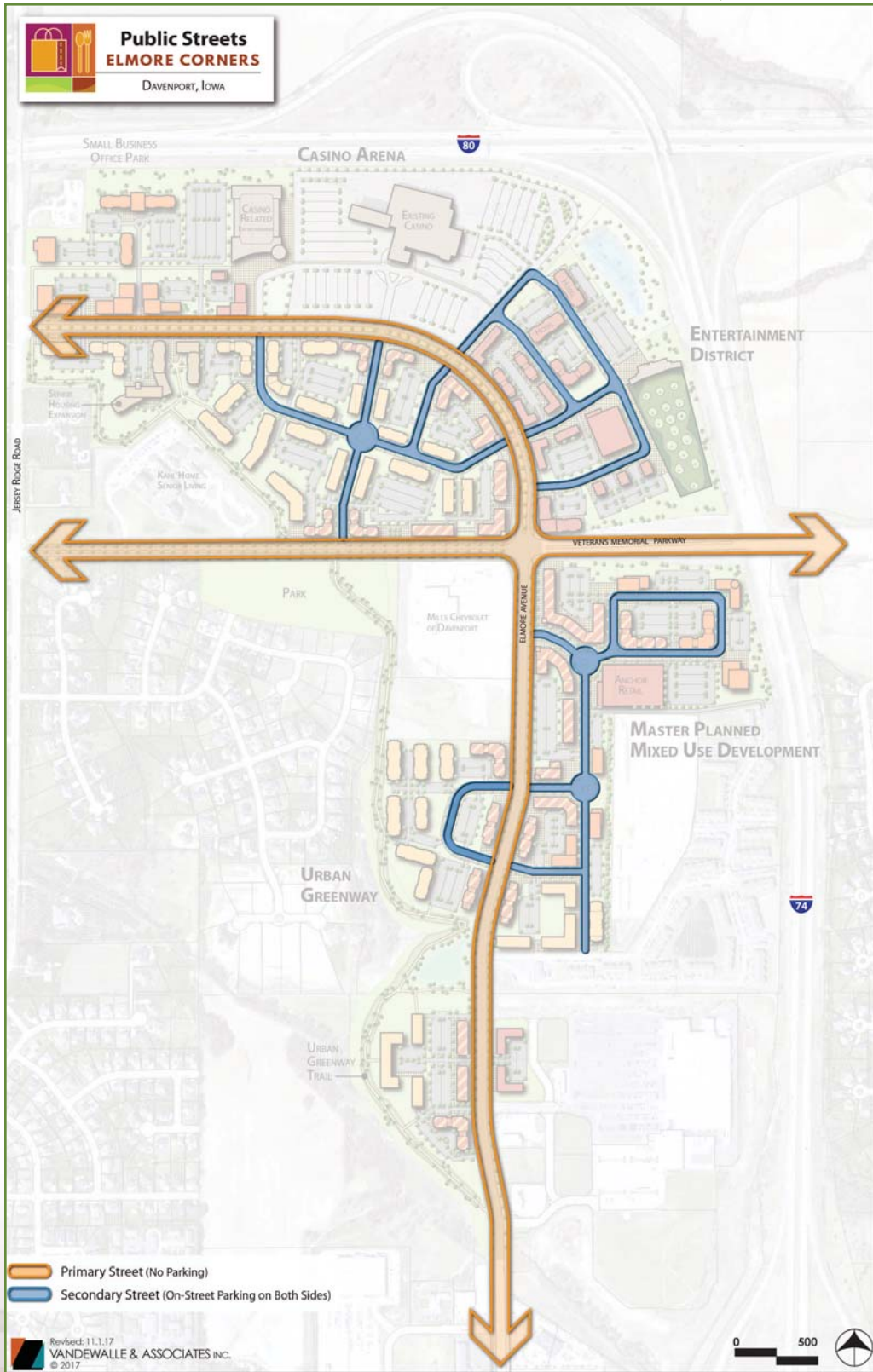




CIRCULATION

Maps 5 & 6 illustrate the circulation vision for Elmore Corners. Map 5 highlights public street types, while Map 6 illustrated pedestrian circulation and streetscape areas.

Map 5. Public Streets



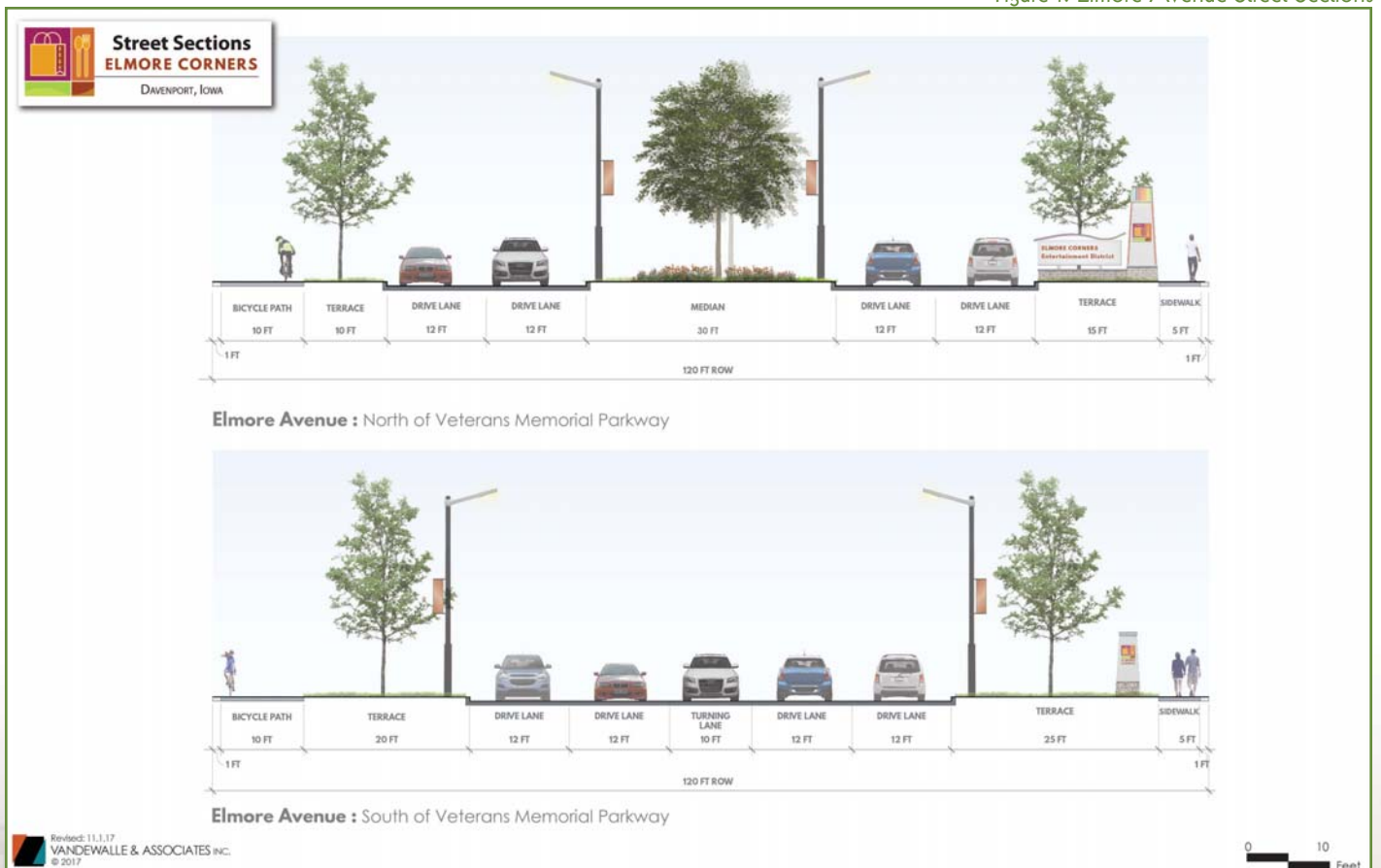
AUTOMOBILE CIRCULATION

The planning area contains two existing public streets, Elmore Avenue running north/south and Veterans Memorial Parkway running east/west. During this planning process, Veterans Memorial Parkway was under construction within the planning area. These streets are designated as Primary Streets on Map 5. Both streets are four-lane streets with two travel lanes in each direction and no on-street parking. To reduce traffic on these two streets, better connect future uses, reduce block size, and provide pedestrian scale streets, additional public streets are proposed. These streets are designated as Secondary Streets. These streets allow access into the existing large blocks, which can increase development intensity and tax base. Secondary streets should have parking on both sides to serve commercial and residential uses, as well as, calm traffic speeds. Terraces in these areas should be considered for stormwater management in the form of stormwater planters and rain gardens.



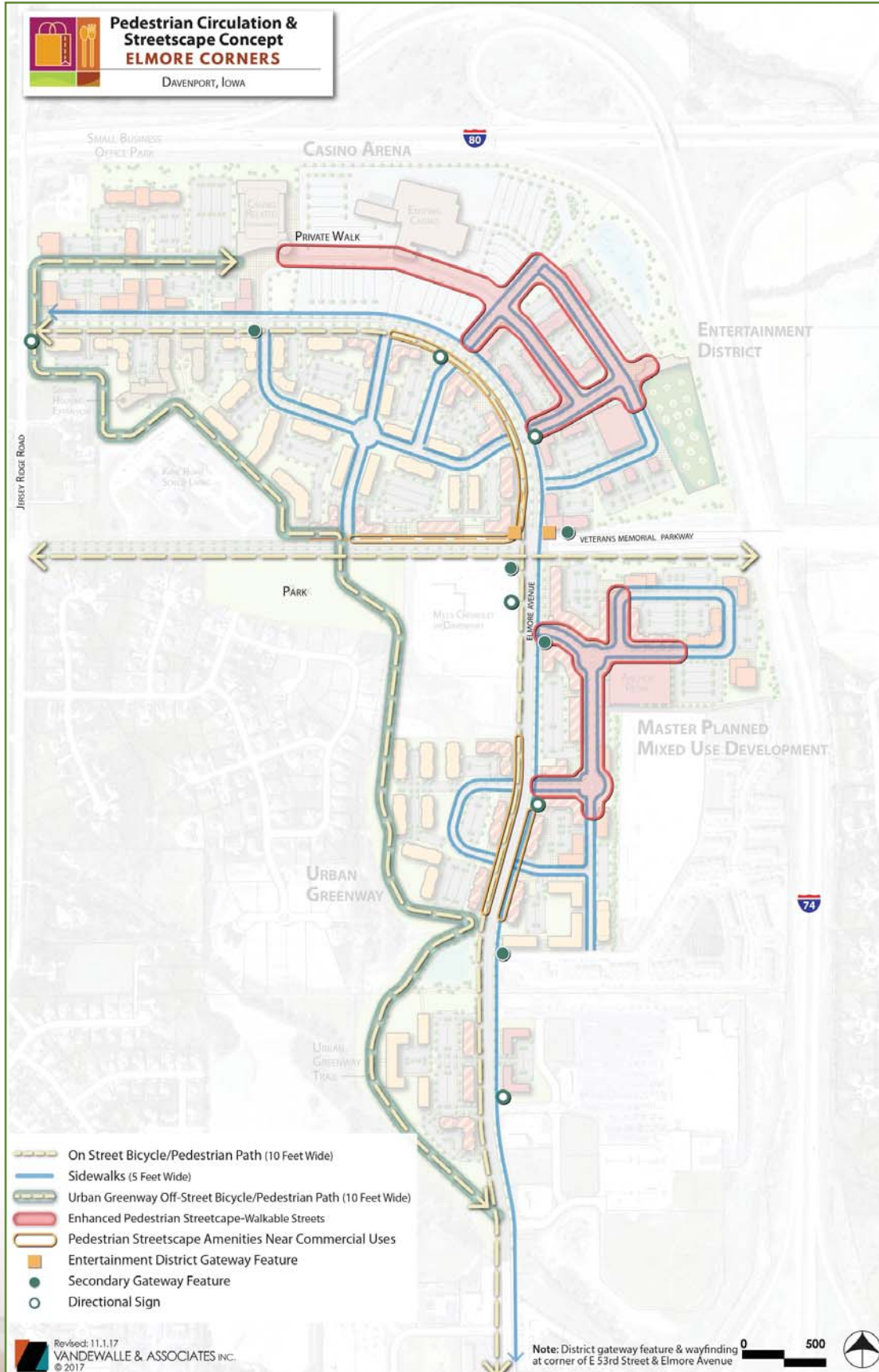
Figure 1 provides a streetscape vision for Elmore Avenue. The street section north of Veterans Memorial Parkway contains a median, allowing for landscaping. This space could be a combination of climax trees to provide street shade, ornamental trees to provide spring and fall interest, native grasses to provide fauna habitat, and potentially rain gardens to collect and infiltrate street stormwater runoff. The medians may also contain street lights to illuminate the road surface. Median street lights are often found in special districts, highlighting the unique Entertainment District. These tall-mast lights should contain large banners celebrating the Entertainment District and special events. Terraces are proposed as turf grass with shade trees and space for wayfinding.

Figure 1. Elmore Avenue Street Sections



BICYCLE/PEDESTRIAN CIRCULATION

Map 6. Pedestrian Circulation & Streetscape Concept

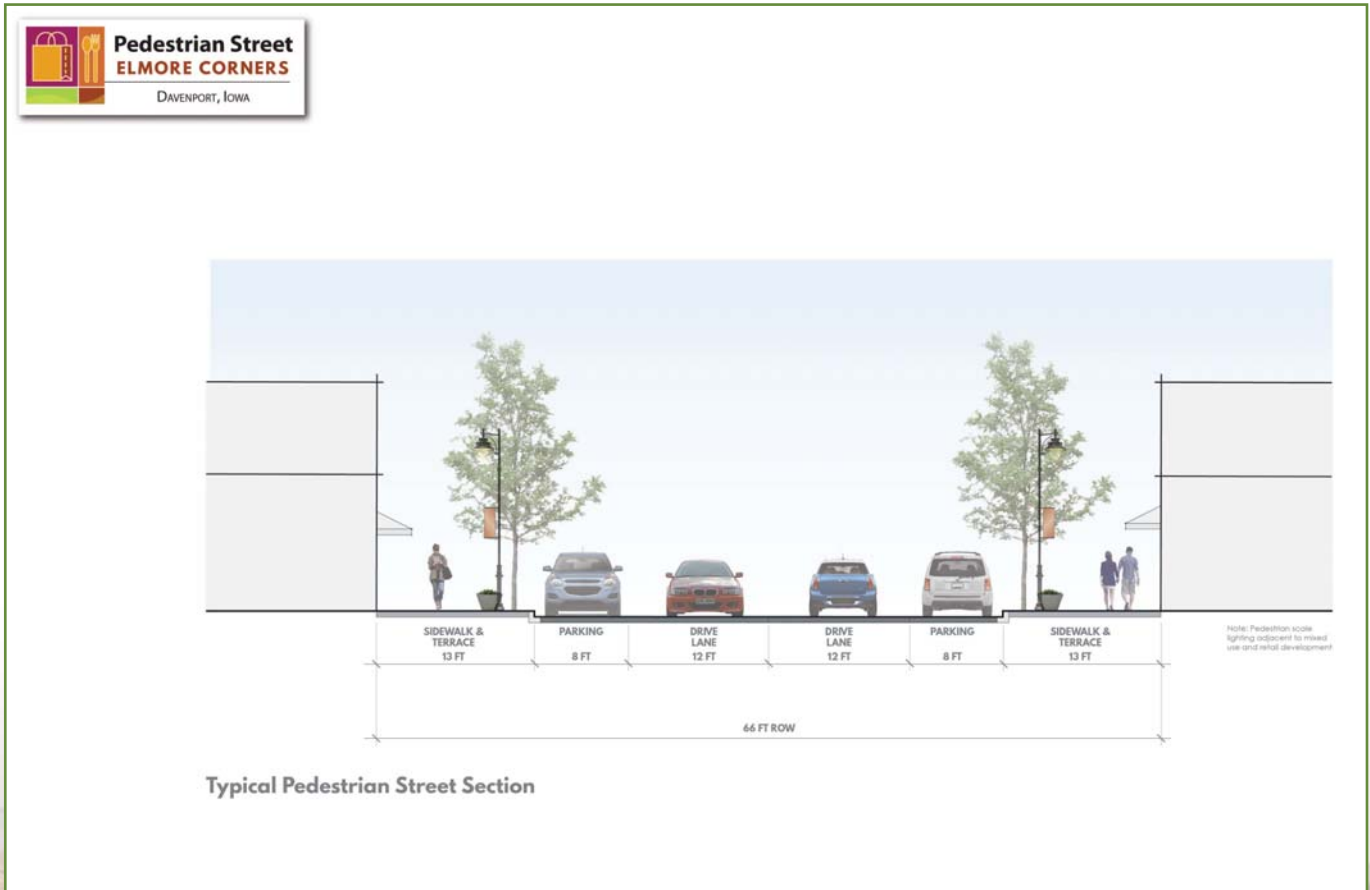


BICYCLE/PEDESTRIAN CIRCULATION

The existing two public streets both contain a sidewalk on one side and a bicycle/pedestrian path on the other side, providing non-motorized circulation options within the planning area. In addition to these facilities, sidewalks are proposed on future streets and a path is proposed within the Urban Greenway, creating a connected pedestrian network. Map 6 illustrates locations for enhanced streetscape areas to create Walkable Streets. These spaces are along secondary streets and have urban cross sections with combined, paved sidewalks and terraces. Figure 2 illustrates a typical street section for these pedestrian streets. The drawing highlights pedestrian scale street lights to illuminate sidewalks, planters, and banners to announce events or destinations. Buildings in these areas are encouraged to interact with the public streetscape by providing outdoor seating and entryways that integrate into the public rights-of-way. Map 6 also suggests location for a wayfinding system, which is explained in the next document section.



Figure 2. Pedestrian Street Section

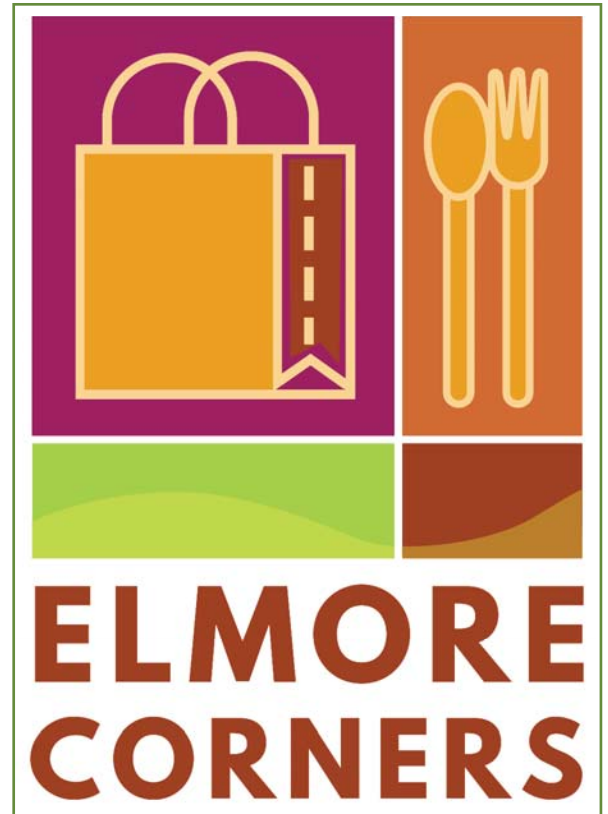


IV. Branding Concept

LOGO

The City created a logo to brand and celebrate the Elmore Corners district. The logo is a graphic representation of an energetic new destination with a range of amenities to attract visitors and serve residents. The following describes the logo design in more detail.

- The four quadrants and the white lines that define them represent the urban corners created by the intersection of two arteries—Elmore Avenue and Veterans Memorial Parkway. The lines are offset to add a dynamic quality
- The upper left corner shows a shopping bag representing retail services
- Upper right bears a fork & spoon representing food and dining—suggesting this is a family-friendly place to spend time, and more than a one-stop destination
- The lower two quadrants show a rolling topography representing the nearby greenspace and path amenities available, and suggesting this will be an attractive place that integrates into its environment. The curving line is also reflective of the casino roof
- Colors were chosen to be fresh and modern while relating to food, nature, and quality built environments. Specifically, berry and pumpkin evoke appetite and foodstuffs; green evokes healthful foods and nature; and terra cotta suggests earth and brick. The split-complementary color scheme (berry-orange-green) adds pop and vibrancy



SIGNAGE

A wayfinding package was developed to celebrate Elmore Corners and direct visitors to destinations. These series of signs identify both the district and destinations within the area.

Gateway Sign

This monument sign is proposed at the intersection of E. 53rd Street and Elmore Avenue to announce the district to the north. The design takes design cues from the Rhythm City Casino. The base is proposed to have stone matching the casino building base material. The top is a curved metal band similar to the casino roofline. The sign materials are primarily stone and metal to reflect the modern Elmore Corners District.

Entertainment District Sign

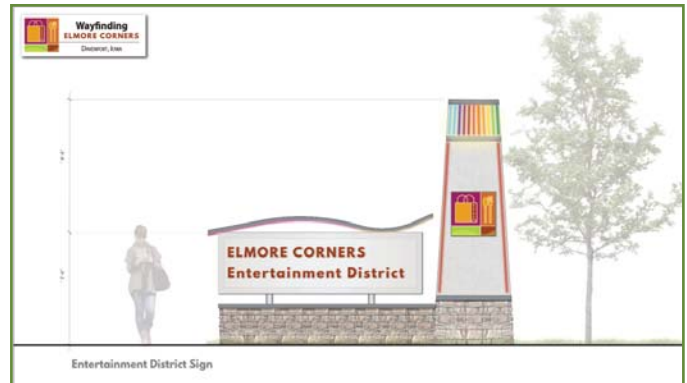
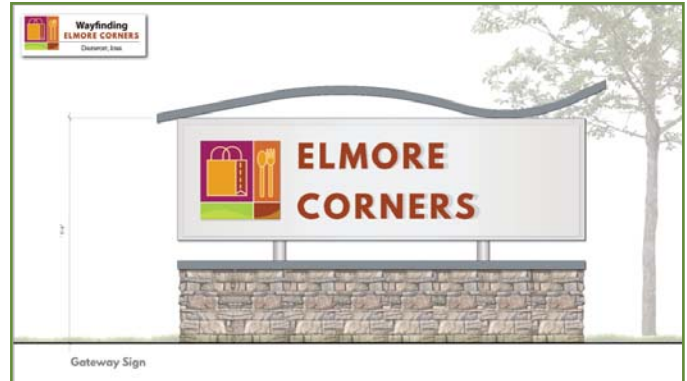
This monument sign is proposed at the intersection of Veterans Memorial Parkway and Elmore Avenue to announce the proposed entertainment district around the casino. The design takes further design cues from the Rhythm City Casino. The left half contains a stone base and curved metal top, similar to the gateway sign, though a band of neon light highlights this curve. The right side of the sign is a pillar of similar materials with the district logo and bands of neon light, matching the casino entrance.

Secondary Sign

Due to the large size of the Elmore Corners District, additional identification signs are recommended throughout the area. These smaller, columnar signs should be located at entryways from all directions along Elmore Avenue and Veterans Memorial Parkway. The design uses similar materials to the other gateway signs.

Directional Sign

The final sign type within the wayfinding package is a pole mounted metal sign directing visitors to specific destination within the district. The sign contains the district logo and a list of destinations with arrows pointing the travel direction. Examples of potential destinations may be buildings such as the Rhythm City Casino, wellness center, or water park resort, or may be a larger development project such as a shopping destination or business park.



V. Design Development Standards

BUILDING DESIGN GUIDELINES

BUILDING COMPOSITION

Guideline: Buildings shall be of a quality design that contribute to the Elmore Corners destination appearance.

Base, Middle, Top

Building facades shall be composed to define base, middle and top elements. The base of the building anchors it to the ground and is the interface between the building and people. This element shall be the most articulated. The transition between the middle of the building and the base and top shall be varied by use of materials, composition, repetition or ornamental features. The top terminates the building against the sky and provides an opportunity to create an interesting silhouette.

Rhythm

Rhythm refers to the regular or harmonious recurrence of building elements. Rhythmic elements need not be boring, but can provide the visual “glue” that links together various buildings on a street. These patterns commonly reflect the building’s repetitive structural bays, often with the end bays given special identity. This articulation of the facade helps provide scale by comprising the facade into smaller visual parts. The variation of rhythm from building to building reaffirms the individuality of each building, while the recurrence of an overall rhythm helps unify the facade. The building’s structural column lines shall in some way be evident or expressed on its facade.

Scale

In general, distinctive compositional elements of buildings (entries, structural bays, roof elements, windows) shall be distinguishable from both near and far distances. The size and shape of these elements shall give the building scale to relate to pedestrians as well as surrounding buildings. Special attention shall be paid to overall building height and massing in order to maintain a scale and character compatible with the area.

Massing

Large buildings shall be comprised of a hierarchy of masses and forms to give the building a more human scale and visual richness. Techniques include using designed recesses or projections, creating distinct building components, and varying occasional roof forms according to individual building components. However, excessive use of changes in form can create a decidedly confused street façade and disharmony.



Proportion

Building massing and components shall demonstrate consistent proportional harmonies. Typically, pedestrian friendly streets have building elements that rely more on vertically proportioned elements than horizontally proportioned elements. While no architectural rule is always valid, the use of vertically proportioned facade components seems to offer a traditionally street-friendly composition (regardless of specific architectural styles). The use of proportion is intended to provide a sense of visual harmony among elements of a building.

Architectural Form

- Buildings shall be designed to create a pedestrian friendly and scale neighborhood
- Buildings shall be primarily rectangular in form and sited parallel to public streets
- Buildings are encouraged to have a mix of tenants and uses
- Entrances shall be provided at public streets and on parking sides
- Connect building entrances to public sidewalks and pedestrian pathways
- Articulate building shapes with window bays, cornices, parapets, and towers
- Buildings shall have primarily flat roofs with varied or broken lines
- Roof mechanicals shall be screened
- Locate service areas away from public streets and provide architectural screening
- Drive through facilities shall be located on rear building façades as to not dominate the streetscape
- Modify franchise architecture to fit the desired Elmore Corners character

Inappropriate Design

- Flat, massive, monolithic geometric shapes
- Plain, undersigned buildings with large blank spaces
- Small buildings with single occupants
- Gabled, hipped, or steeply pitched roofs
- Large, monotonous roof forms
- Visible building mechanicals
- Faux-historic buildings

Building Composition



BUILDING FAÇADE

Guideline: Buildings shall have varied facades composed of high quality lasting materials.

- Façades shall be articulated on all sides to provide visual interest.
- Break up façades and create visual interest and pedestrian scale by:
 - Varying materials and colors
 - Varying façade vertical and horizontal setbacks
 - Incorporate architectural reveals or floor step backs
 - Vary window styles and moldings
 - Design enhanced architectural details on the ground level
- Façades on multiple-tenant buildings shall be broken into design elements that reflect individual tenant spaces through vertical façade plain staggering, window/door groupings, and awnings
- Differentiate ground and above floors through horizontal banding, signage bands, and windows
- First floor façades shall be transparent to allow retail uses to interact with the street and parking-side entrances
- Commercial and retail façades shall have at least 40% transparency on the street façade
- Secondary façades that do not face the public street or parking areas are encouraged to have transparent windows. If this is not possible, then façades shall contain architectural features that provide interest
- Building at corners shall contain interesting architectural features that address the corner such as towers, entrances, and metal or cloth awnings
- Identify building entrances through permanent awnings, overhangs, or signage

Example Façade Design



Inappropriate Façade Treatment

- Large unarticulated wall planes
- Large expanses of walls without windows
- Projecting wall-mounted mechanical units
- Vinyl awnings
- Buildings with no pedestrian scale or defined entryways



Materials

- High quality materials shall be installed on buildings to promote Elmore Corners
- Long lasting materials such as brick and stone shall be used to withstand weather and provide a feeling of permanence
- Primary building materials shall be natural stone, masonry, brick, and glass and be of natural earth tone colors
- Secondary materials such as decorative metal, wood, stucco, and cement board may be incorporated
- Using a mix of materials is recommended to create visual diversity on the building façade
- Enrichment of the pedestrian realm requires building materials at the street level to be of high quality:
 - These could include items such as brick, stone, decorative concrete masonry units, metal panel systems, or other creatively used high quality and durable building materials
 - Decorative finished block systems can be used along the base of the buildings, and as an accent, but are not recommended as the dominant building material on the entire building
 - Utility grade materials shall only be used on facades of the building not visible from publicly accessible areas
- Freestanding commercial structures shall be designed as four-sided architecture with finish grade materials used consistently on all façades:
 - Recommended primary materials include brick, stone and glass
 - Other materials such as precast concrete, decorative concrete block or decorative facade panels may be appropriate if properly detailed and integrated with the architecture
 - Metal and finished wood may be used as accents, but shall not be the primary material for any façade

Example Materials



Inappropriate Materials

- Unfinished exposed concrete block
- Large, unfinished, pre-cast concrete panels
- Unfinished poured-in-place concrete
- Corrugated metal, vinyl, or aluminum siding
- Plywood siding panels
- Exposed treated lumber
- EIFS as a primary material or located on the first floor
- Windows
 - Moderate to highly reflective glazing
 - Strongly colored or darkly-tinted glazing
- Colors
 - Predominant use of bright, high intensity colors
 - Predominant use of metallic and neon colors



SETBACKS

Guideline: To provide a continuous street edge along the Elmore Corners District, buildings shall be sited within setback areas unless not possible due to affecting business operations.

- Along Elmore Avenue and Veterans Memorial Parkway, the primary building façade shall be located within 20-40 feet from the property line
- Along secondary streets, the primary building façade shall be located 0-10 feet from the property line
- To maintain an urban edge that is pedestrian friendly, areas adjacent to parking lots shall be screened with enhanced landscaping

Inappropriate

- Buildings set back over 40 feet from public streets
- Parking lots between public streets and the primary façade of a building



DENSITY

Guideline: Develop land as densely as possible to increase tax base while providing adequate landscaping, stormwater management, and connections to public ways.

- Development shall be designed to an urban density with minimal spaces between buildings
- Install advanced stormwater management practices to increase density
- Density may increase through vertical mixed-use and shared parking arrangements



SIGNAGE

Guideline: Signage shall function as a marketing device, complement building architecture and materials, and not dominate the site. All signs must meet City Sign Codes.

Appropriate and attractively designed site signage is an important component of the overall aesthetic appeal of any commercial or mixed-use area. Good signage based on design and graphic quality is encouraged throughout Elmore Corners.

- Business signage shall be wall mounted (projecting or flat), monument (if located away from the building), and window, canopy or awning style
- Similar materials, colors and styles shall be used to ensure the signage is consistent with the building design
- Signs shall reflect and enhance the nature and appeal of the retail and commercial experience and not be just a list of tenants
- Materials shall be of high quality to prevent premature weathering of the sign
- Signs shall fit within and not overwhelm the architectural features of the buildings
- Signs shall harmonize with their surroundings in terms of size, shape, color, texture, and lighting so that they complement the character of the neighborhood
- Signs shall be professionally constructed using high quality materials such as metal, stone, tile, composites, brass/metal plated, hardwood, and glass
- The creative use of materials, lettering, and interesting use of graphics is allowed and encouraged if the signs work well and complement the overall building and street design
- Signs shall be designed with the purpose of promoting retail and street activity while enhancing the pedestrian experience
- Addresses shall be clearly visible from the public right-of-way
- The use of tastefully designed pedestrian scale directories is encouraged for multi-tenant buildings
- Monument signs shall contain a base and not be pole-mounted
- Monument signs shall contain landscaping at the base that will not grow to cover the sign message
- Lighting shall be designed carefully to avoid excessive glare or over illumination:
 - Letters can be illuminated internally or externally
 - External lighting must be discreet and unobtrusive, and not shine outward into the pedestrian or driver vision path
 - Fixtures shall be simple but attractive
 - Lighting signs and letters shall be done in an attractive and subtle technique
 - Individual letters that are backlit, halo-lit, reverse illumination channel letters, and neon are encouraged



Example Signage

Inappropriate Signage

- Pole signs, billboard style wall-mounted and roof mounted signs are not allowed without special permission from the City Plan Commission
- Monument signs advertising individual businesses
- Tall mast signs
- Large temporary signs that impede pedestrian traffic
- Inflatable signs
- Overly illuminated signs
- Unshielded sign light sources, allowing upward transmission of light
- LED, animated, flashing, blinking, and video signs
- Plastic box signs
- Generic, un-stylized signs
- Large temporary signage located in windows that reduces transparency
- Large-scale street-level signage that overwhelms the pedestrian environment
- Signage that obscures architectural details
- Glaring and directed spotlights that affects neighboring properties

ENTERTAINMENT DISTRICT SIGNAGE

Since the area around the Rhythm City Casino is planned for entertainment purposes, sign design and guidelines may be relaxed to create an exciting sense of place. Signs in this area may be taller along the interstate corridors, contain additional illumination, and may have moving components to advertise this district and its activities.



SITE DESIGN GUIDELINES

PARKING LOTS - DESIGN AND LOCATION

Guideline: Parking lots shall be located behind buildings or interior of sites whenever possible and contain landscaping to buffer views from public ways.

- Parking lots shall be designed to allow for ease of vehicular circulation and meet all parking lot requirements set in the City Zoning Code
- Locating parking lots at corners shall be avoided, when possible
- Safe pedestrian ways shall be provided between parking spaces and building entrances
- Canopy trees shall be planted at a minimum of every 12 parking stalls to reduce heat islands
- Edge landscaping shall be installed to screen parking lots from public streets
- Planting areas shall be incorporated into landscape islands and planting areas between parking bays
- Bio-swales and stormwater best management practices shall be used to collect and infiltrate stormwater run off
- Parking lot lights shall be 18-25 feet in height along pedestrian ways and edges
- Tall pole lights may be installed in parking lot internal bays with a maximum height of 30 feet
- Site lighting along pedestrian walkways is encouraged in bollards to provide safe lighting, but not over-illumination
- Lighting shall cast downward and be full cut-off
- Illumination levels shall not exceed 0.5 foot candles at the property line
- Businesses shall provide bicycle parking
- Short-term bicycle parking areas shall be provided within 20 feet of each commercial primary building entrance when possible
- Long-term bicycle parking areas shall be provided within 100 feet of each commercial primary building entrance when possible
- One bicycle parking space per 2,000 gross square feet shall be provided for retail uses. A minimum of two spaces is required
- One bicycle parking space per 10,000 gross square feet shall be provided for office. A minimum of two spaces is required
- A minimum of one bicycle parking spaces per unit shall be provided. In-building or covered bicycle parking is encouraged

Inappropriate Parking Lot Design

- Parking lots located between buildings and public streets
- Parking areas located on corners
- Individual parking lots for each development/business
- Numerous access drives into disconnected parking areas
- No parking lot landscaping
- No bicycle parking



PARKING MANAGEMENT

Guideline: Connect parking lots between developments and share parking between uses to minimize large parking lots and automobile traffic on public streets.

- Parking management shall promote effective economic activity, maximize the sharing of parking areas for commercial activity, and avoid inefficiencies in parking in which parking occupancy patterns discourage higher density and compact development
- Shared parking strategies are encouraged to eliminate redundant and unnecessarily large parking areas and to provide more land for taxable development. Parking lots are encouraged to be shared between building uses that require parking at separate times such as office parking during the day and residential parking at night
- New uses shall be required to produce a parking design and management study which documents the level of use at regular and peak times, the degree of sharing for parking with nearby users, the way in which the owner/user intends to maximize efficient use, and sharing of the parking
- Cross access and/or joint parking easements may need to be developed in the event of the parcel being subdivided.
- Shared driveways shall be used to serve groups of buildings with shared parking courts between buildings
- Sharing arrangements shall be codified in covenants, deed restrictions, and developer agreements
- Connecting parking lots between development lots is encouraged to reduce traffic congestion on public streets

Inappropriate Parking Management

- Individual parking lots by building, use, or development project
- Parking lots that are overbuilt and unused



SITE CIRCULATION

Guideline: A pedestrian friendly environment is encouraged to provide safe circulation within the site and between sites. Private development shall connect into existing public infrastructure.

- Concrete pedestrian walkways shall connect parking areas to building entrances
- Pedestrian walkways shall connect development sites to existing public sidewalks and paths such as the Urban Greenway
- Walkways shall be located outside of vehicle parking areas
- Walkways shall be safely illuminated

Inappropriate Site Circulation

- Unpaved pedestrian walkways
- Parking lots with no pedestrian walkways
- Dark, unlit walkways



DRIVE-THROUGH AND SERVICE

Guideline: Drive-throughs shall be located at the rear or side of buildings and shall not be placed between a public street/easement and the main building structure.

- Every effort shall be made to coordinate and integrate drive-through facilities into the overall architectural treatment of the main building
- Creative design solutions such as remote kiosks are encouraged to minimize the impact of the drive-through facility on the overall site design
- Clearly defined pedestrian crossings shall be provided where walkways intersect drive-through access lanes. In all cases drive-through facilities shall be designed to operate without endangering the public safety

Inappropriate Drive-Through

- Drive-through window and service line visible from public streets



SERVICE, STORAGE, AND UTILITY AREAS

Guideline: Service, loading, and utility areas, while necessary for business operation, shall be located away from the public view.

- Refuse and recycling areas shall be integrated into the building architecture with high quality design and/or screened with similar building materials to the main architecture. The areas shall be kept clean and avoid accumulation of excess disposal materials and the enclosure shall be locked
- When possible, service and utility areas shall be inside the building or integrated into the architecture of the building
- Loading areas shall be located behind buildings, away from parking areas and public streets
- Loading doors shall not be visible from public streets
- To reduce the amount of area dedicated to these needs, shared service areas between adjacent users and buildings shall be allowed and encouraged
- If indoor reuse is not possible, outdoor refuse areas shall be enclosed with attractive design and materials that match building architecture. Recommended enclosures would be of masonry or stone with gated access and surrounding landscaping. The gate and enclosure opening shall not be visible from public ways
- Rooftop mechanicals shall be screened with materials that match the building architecture

Inappropriate Service Area

- Service areas and utility equipment along public streets
- Prefabricated storage sheds
- Visible garbage dumpsters
- Unscreened service, storage, or utility areas



LANDSCAPING

Guideline: Create pedestrian-friendly landscapes that are functional, visually appealing, and relate to building activities and architecture. Use landscaping to screen undesirable views.

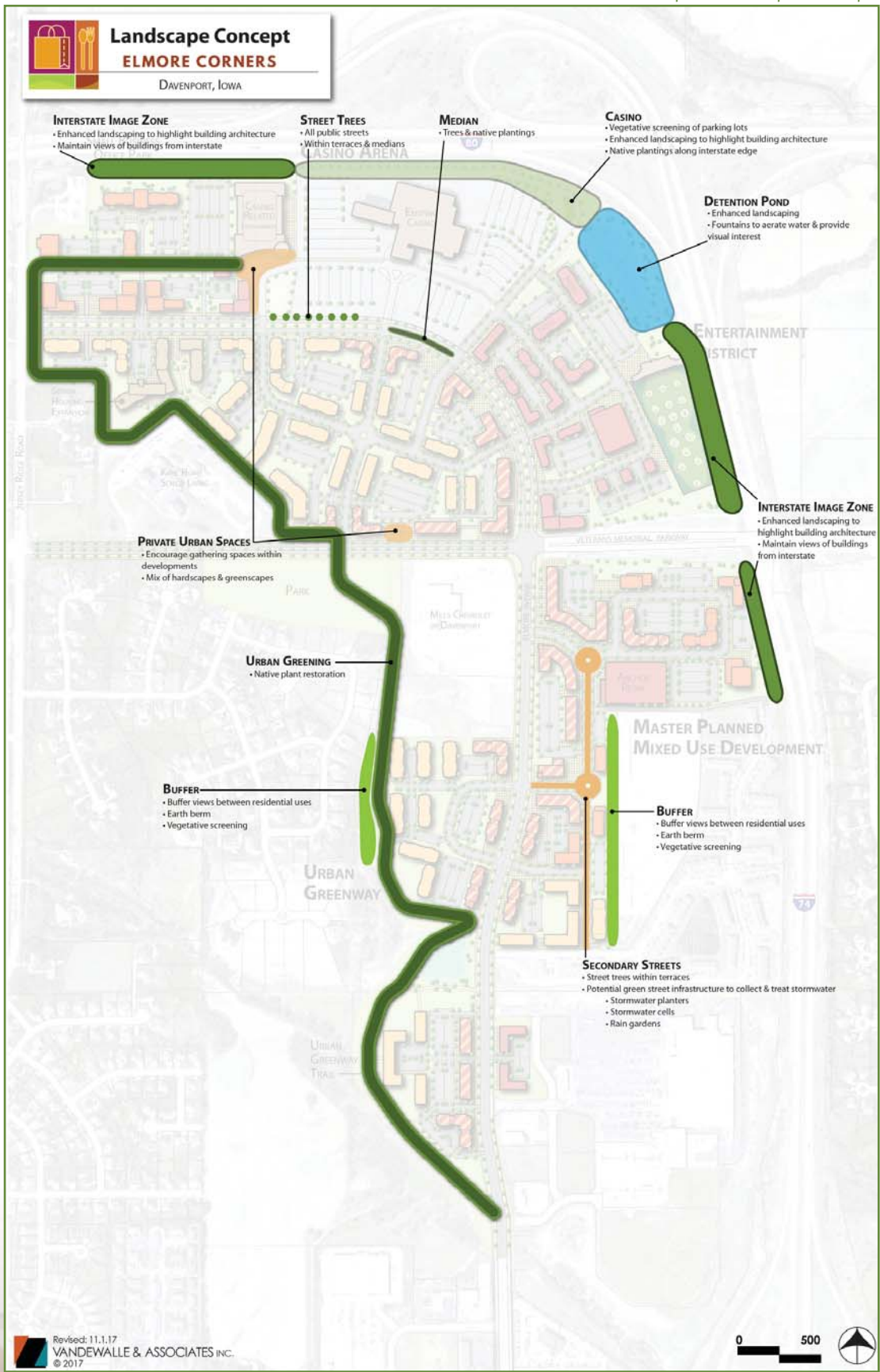
- Incorporate landscape elements that complement the character of the building and provide a pleasing relationship with adjoining properties, the public sidewalk, and the street
- Use appropriate landscape elements to establish continuity between buildings and to define the block face where there are no buildings
- Plant shade trees in surface parking lots to reduce heat islands
- Incorporate mid-level plantings and ground covers into parking planting areas
- Native plants shall be installed whenever possible
- Consider prairie plantings instead of turf to minimize maintenance and establish animal and insect habitat
- Use plant materials that provide year-round interest, add desirable color, texture, and smells to a site's experience
- Install plants to screen parking areas from public streets.
- Install plants to screen building mechanicals
- Incorporate rain gardens and bioretention basins to collect runoff and filter pollutants
- Install bioretention areas in surface parking lots for stormwater infiltration
- Use plant materials that are compatible with urban environments and will maintain their health with the expected amount of care
- Use salt tolerant plants due to the urban pedestrian environment
- See Map 7 for a district-wide landscape concept

Inappropriate Site Landscaping

- Single species planting schemes
- Residential or suburban landscape treatment such as boulders, bark chips, plastic edging, railroad ties, etc.
- Chain link, metal mesh, wire or barbed wire fencing
- Site furnishings located adjacent to common spaces



Map 7. Landscape Concept



URBAN GREENWAY

Guideline: An urban greenway shall be established on the west edge of the planning area to both buffer new development from existing neighborhoods and provide an open space corridor for recreation and natural habitat.

- The greenway corridor shall be a 100 foot wide minimum public corridor
- Naturalized landscaping shall be installed to establish the natural space and buffer
- Low-maintenance native species including trees, shrubs and grasses shall be planted
- A 10 foot wide asphalt path shall be constructed to provide an alternative transportation route and recreation trail
- The path shall connect to private development, public streets, and sidewalks



SITE FEATURES & URBAN SPACES

Guideline: Private development is encouraged to establish urban spaces and install site features to enhance the Elmore Corners District and contribute to a pedestrian friendly streetscape.

- Commercial uses are encouraged to have public plaza spaces to provide space for activities such as outdoor dining or seating areas
- Commercial urban spaces shall be accessible from public ways
- Any security fencing shall consist of decorative materials (such as wrought iron, brick or stone)
- Select and locate site furnishings (benches, trash receptacles, bicycle racks, etc.) to unify the Elmore Corners District and provide a pleasing relationship with adjoining properties, the public sidewalk, and street
- Site furnishings shall be of high-quality materials that last in all weather environments

Inappropriate Site Features & Urban Spaces

- Urban gathering spaces that are closed off from public ways
- Site features of low quality that do not complement building architecture



PUBLIC STREETS AND STREETSCAPING

Guideline: Public streets shall be designed as complete streets serving all modes of transportation users including vehicles, bicycles, and pedestrians. Streets shall contain streetscape elements to enhance the visual aesthetics of these public spaces.

- Primary public streets shall contain streetscape elements of a vehicular scale including:
 - Street lights – tall poles to safely illuminate the street
 - Sidewalk on one side and bicycle path on the other side
 - Turf terraces
 - District gateway features
 - Wayfinding sign directing visitors to district destinations
 - Light pole banners advertising special events
- Secondary streets shall contain streetscape elements of a pedestrian scale including:
 - Street lights – pedestrian scale with decorative fixtures
 - Concrete sidewalks on both sides of the street
 - Decorative brick, paver, or stamped concrete terraces
 - Wayfinding signs or kiosks directing visitors to district locations
 - Light pole banners advertising locations, uses, or events
 - Landscape planters with flowers and grasses
- All street lights shall contain LED energy efficient fixtures
- Street lights shall contain decorative poles and bases
- Street lights shall be down casting to reduce light pollution
- Stormwater management could be incorporated into the street terrace through stormwater planters, inlets, and swales
- Secondary streets may designate bicycle access through painted sharrows on street lanes



STORMWATER MANAGEMENT

Guideline: All future developments must meet City Stormwater Ordinance requirements. In general, stormwater best management practices are encouraged and shall be incorporated into site designs, limiting large, monolithic retention ponds.

- Incorporate stormwater systems into development site plans and landscape plans
- Encourage regional ponds or ponds that serve multiple developments to density development and allow development to occur in urban form
- Utilize stormwater management techniques that maximize infiltration and filter runoff
- Incorporate bio-swales into parking lots to collect surface water and infiltrate on-site
- Install urban stormwater management systems where appropriate such as stormwater planters, rain gardens, underground cisterns and cells, porous pavement, and green street terraces
- Green roofs are encouraged to aid in reducing the amount of impervious surfaces
- Collect roof water into “purple pipe” grey water systems to reuse for irrigation, toilets or other non-potable uses
- If water does run off buildings, direct it into planting areas and rain barrels
- Include stormwater pond aeration systems and fountains to improve water quality and appearance

Inappropriate Stormwater Management

- Direct stormwater runoff into the Pheasant Creek
- Large, unnaturally shaped stormwater detention ponds with no landscaping or aeration



HIGHWAY CORRIDOR OVERLAY DISTRICT

This planning area lies within the City of Davenport Highway Corridor Overlay District. Project proposed within this area must meet the requirements set in the City Zoning Code.

PREFERRED LAND USES

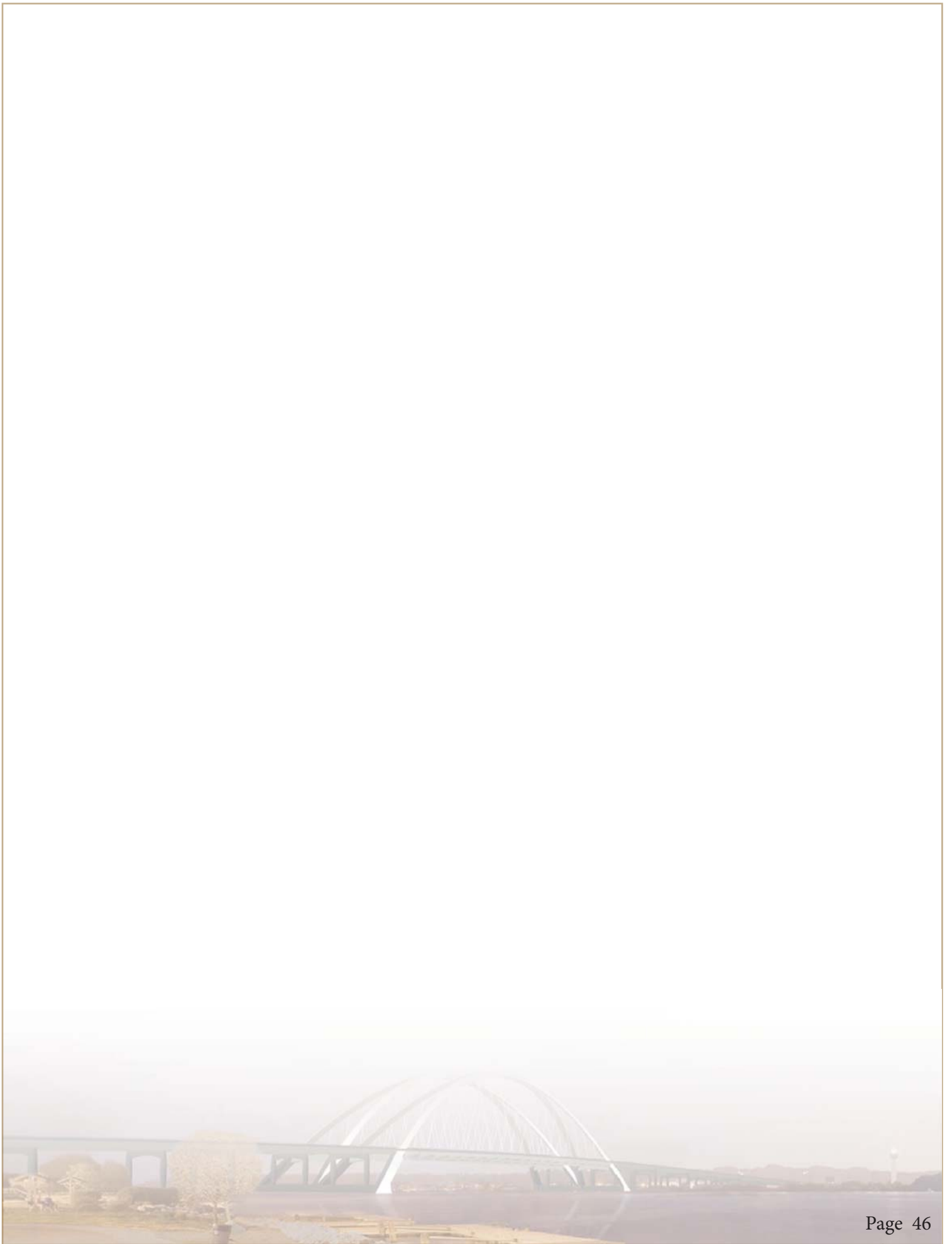
Table 1: Elmore Corners Preferred Land Uses lists the currently allowable uses within the planning area based on the underlying Planned Development District Zoning. The chart indicates the preferred uses by area based on the Elmore Corners Site Layout Plan vision. The area numbers correspond with the following subareas (see Map 4. Sub Areas on page 19).

- Area 1 - North End
- Area 2 - Elmore Corners Mixed-Use Neighborhood
- Area 3 - Elmore Corners Entertainment District
- Area 4 - Master Planned Development
- Area 5 - Elmore Corridor

Table 1. Elmore Corners Preferred Land Uses

Elmore Corners Preferred Land Uses					
Area	1	2	3	4	5
Planned Development District					
Principal Uses					
Banking facilities	Y	Y	Y	Y	Y
Business parks	Y	N	N	N	N
Hotel and motels	Y	N	Y	Y	N
Indoor recreation uses	Y	N	Y	N	N
Indoor recreation uses and outdoor uses that relate to other activities developed on the site	Y	N	Y	N	N
Medical, dental, research testing, and experimental laboratories	Y	Y	N	Y	Y
Offices	Y	Y	N	Y	Y
Restaurants and food services	Y	Y	Y	Y	Y
Retail sales of merchandise	Y	Y	Y	Y	Y
Retail sales that do not include vehicle sales or rental, or public garages	Y	Y	Y	Y	Y
Condominiums	Y	Y	N	Y	Y
Independent, assisted and congregate care elderly housing	Y	Y	N	N	Y
Apartments	Y	Y	N	Y	Y
Row houses	Y	Y	N	Y	Y
Single-family attached	Y	Y	N	Y	Y
Single-family detached	N	N	N	N	N
Warehouse distribution uses (provided that all storage activities are carried out completely within building)	N	N	N	N	N
Other Uses Not Listed in Planned Development Districts					
Grocery store and supermarkets	N	N	N	Y	N
Outdoor storage	N	N	N	N	N
Large format retail over 40,000 square feet	N	N	Y	Y	N
Legend					
Y	Preferred				
N	Discouraged				







CITY OF DAVENPORT
 Development & Neighborhood
 Services – Planning
 1200 E. 46th St
 Davenport, IA 52807

Office 563.326.6198
 planning@davenportiowa.com

APPLICATION FOR
DESIGN REVIEW

DESIGN REVIEW BOARD

REVIEW DISTRICT	SUBMITTAL DATE	MEETING DATE

SITE ADDRESS & PARCEL OR LOCATION DESCRIPTION

APPLICANT INFORMATION

Applicant Name | Company Name
 Address
 City | State | Zip
 Phone
 Secondary Phone
 E-Mail Address

BRIEF OVERVIEW OF THE PROJECT (not a scope of work)

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ALL SUBMITTALS SHALL INCLUDE: **SUBMITTED**

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 all work & materials shall be described & itemized/listed in detail
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 rendering as proposed & depicted on existing building/signs
 Material specifications: type, dimensions, & color
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DEVELOPMENT TEAM

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Address
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 E-Mail Address

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Project Manager/Other

Address
 Phone Secondary Phone
 E-Mail Address

Submit this form with attachments to: planning@davenportiowa.com

Design Review Board (DRB) | City of Davenport Meeting Calendar 2026

The Applicant and/or their representative MUST attend the Meeting

Submittals and certifications are to be submitted to Davenport Public Works, 1200 E 46th St, Davenport, IA 52807.

Submittal/Application Deadline**	Meetings***
12:00 P.M. on the Friday Identified Below	5:00 P.M. on the Monday Identified Below Applicant/Representative Presence Required
01/16/26	01/26/26
02/13/26	02/23/26
03/13/26	03/23/26
04/17/26	04/27/26
05/15/26	05/26/26
06/12/26	06/22/26
07/17/26	07/27/26
08/14/26	08/24/26
09/18/26	09/28/26
10/16/26	10/26/26
11/13/26	11/23/26
12/11/26	12/21/26

* Change Due to Holiday.

** Email completed PDF Application to: planning@davenportiowa.com, or mail USB drive with a copy of the completed application to: Planning, Davenport Public Works, 1200 E 46th St, Davenport, IA 52807.

*** All meetings are held at Davenport City Hall, 226 W 4th Street, Davenport, IA.
Applicant/Representative Presence Required

For more information contact: planning@davenportiowa.com or 563.326.6198.

Any and all Date/Location/Time are subject to change.

City of Davenport

Department: Development & Neighborhood Services
Contact Info: Matthew Reu | 563-888-2221

Action / Date
3/23/2026

Subject:

Case DR26-02: Request for Design Review, C-D Downtown Zoning District; build flood protections for Union Station. 102 S. Harrison St. City of Davenport, petitioner. [Ward 3]

Recommendation:

Staff recommends Case DR26-02 be approved in accordance with the submitted work write-up and materials.

Background:

Union Station is located in the C-D Downtown Commercial Zoning District and subject to routine flooding. The City of Davenport is requesting design review for planned site changes to Union Station. Namely, the plan calls for earthen embankments combined with a floodwall to prevent flood water from reaching the building. Second, the wall will be accompanied by an increase in landscaping. Lastly, the parking lot material and circulation pattern will change to alter the flow of water, improve access, and improve driver safety when visiting Union Station. The Design Review Board is asked to review the landscaping, the flood wall, and the redesigned parking lot. Signage is approved separately.

Major changes outlined

- New landscaping
- New earthen berms
- Install brick-faced flood wall
- Install removable floodgate
- Remove brick parking lot and install concrete parking lot with flood vents
- Redesign parking circulation for safety

Landscaping

Much of the current landscaping will be removed prior to construction of the flood protection. New landscaping will contain ornamental trees, deciduous shrubs, evergreen shrubs, ornamental grass, native seed, and low mow fescue seed, and other perennials. On the southwest and southeast corners of the lot, earthen berms will ascend from the south, the railroad tracks, to provide more protection from floodwaters. The earthen berms are meant to augment the wall.

Floodwall

The floodwall extends from East to West along the southern lot line. The floodwall will be faced with a brick veneer attached to concrete and capped with stone. The wall will be as high as five feet from grade, approximately. Grade lies at 561.87 feet. The regulated floodplain lies at 564 feet. The top of the wall will lie at 565.5 feet, exactly 1.5 feet above the regulated and expected flood level. The floodwall contains one gate on the western driveway onto South Ripely Street.

The gate is approximately four feet tall and 15 feet wide. The gate is made of removable boards. Each board is made of aluminum. The gate will not be closed unless a flood event occurs.

Parking lot changes

The parking lot and parking circulation will change with the approval of the project. Cars will have the opportunity to drive to the rear of the structure, the South. The southern parking spots will be removed and replaced with parking. Drivers can still cross under the building but landscaping will increase safety by persuading drivers to slow down and look before crossing under. The physical material of the parking lot will change too. Currently, bricks make up the parking lot. The proposed project calls for Portland cement concrete. The Assistant City Engineer stated to Plan & Zoning Staff that the change from brick parking lot to concrete is to protect the building and help the wall do its job. In flood events, brick allows water from the ground up, damaging the foundation of Union Station and jeopardizing the City's ability to maintain the structure.

FEMA's NFIP Technical Bulletin 2 - Flood Damage-Resistant Materials Requirement

Because Union Station is within the regulated floodplain, or the Special Flood Hazard Area (SFHA), the project is required to use FEMA approved materials below the SFHA. Therefore, the parking lot is made of concrete, an approved material for floors and ceilings. Where as the brick is not rated to be a floor. The wall, steel in core, is acceptable as a wall and as a floor.

Staff recommends Case DR26-02 be approved in accordance with the submitted work write-up and materials.

C-D Approval Criteria

1. Reinforce historical continuity & unique character
2. Encourage public and private investment
3. Pedestrian-friendly spaces
4. Build positive identity
5. Create safe and comfortable space

Attachments:

1. APPLICATION
2. Color Rendering of Proposed Landscaping
3. Plan Set



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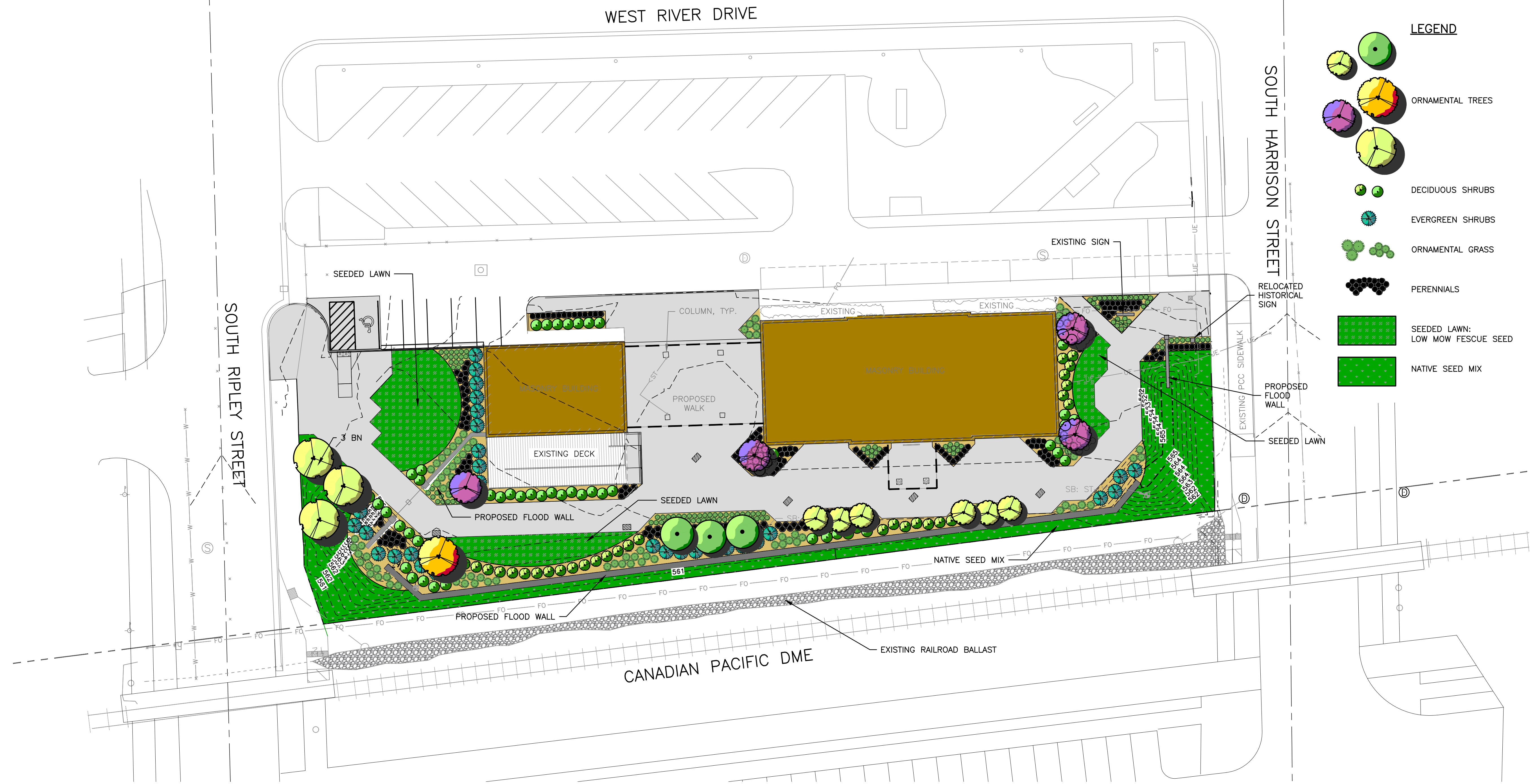
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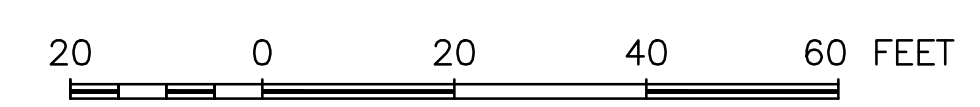
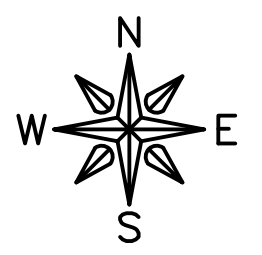
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- LEGEND**
- ORNAMENTAL TREES
 - DECIDUOUS SHRUBS
 - EVERGREEN SHRUBS
 - ORNAMENTAL GRASS
 - PERENNIALS
 - SEEDED LAWN: LOW MOW FESCUE SEED
 - NATIVE SEED MIX



FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
CITY OF DAVENPORT
226 WEST 4TH STREET
DAVENPORT, IA 52801

PROJECT AND LOCATION:
UNION STATION FLOOD REPAIRS
DAVENPORT, IOWA

DRAWN BY: KMT
APPROVED BY: RH
DATE: 03/03/26
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
COLOR RENDERED PLAN

SET TYPE: PRELIMINARY
\\rockford\Drawings\C30\19-717 Davenport Flood Repairs\K.03 COLOR RENDERED PLAN.dwg, COLOR RENDERED PLAN

JOB NUMBER:
19-717PH05

SHEET NUMBER:
K.03

SHEET LIST

Sheet Number	Sheet Title
A.01	TITLE
A.02	LEGEND
C.01	GENERAL NOTES AND ESTIMATED QUANTITIES
C.02	SUPPLEMENTAL SPECIFICATIONS
C.03	EXISTING CONDITIONS
C.04	REMOVAL PLAN
D.01	SITE PLAN
D.02	DETAIL SITE PLAN WEST
D.03	DETAIL SITE PLAN MIDDLE
D.04	DETAIL SITE PLAN EAST
D.05	DETAIL FLOOD WALL PLAN
R.01	SEDIMENT AND EROSION CONTROL PLAN
U.01	FLOODWALL DETAILS
U.02	DETAILS 1
U.03	FLOODWALL DETAILS
K.01	LANDSCAPING PLAN
K.02	LANDSCAPING DETAILS

UNION STATION FLOOD REPAIRS

FOR

CITY OF DAVENPORT DAVENPORT, IOWA



19-717PH05
PRJ2050/CIP68015
SCOTT COUNTY
MARCH 2024

OWNER/DEV	CITY OF DAVENPORT
ADDRESS	226 WEST 4TH STREET DAVENPORT, IA 52801
PHONE NUMBER	563-326-7711

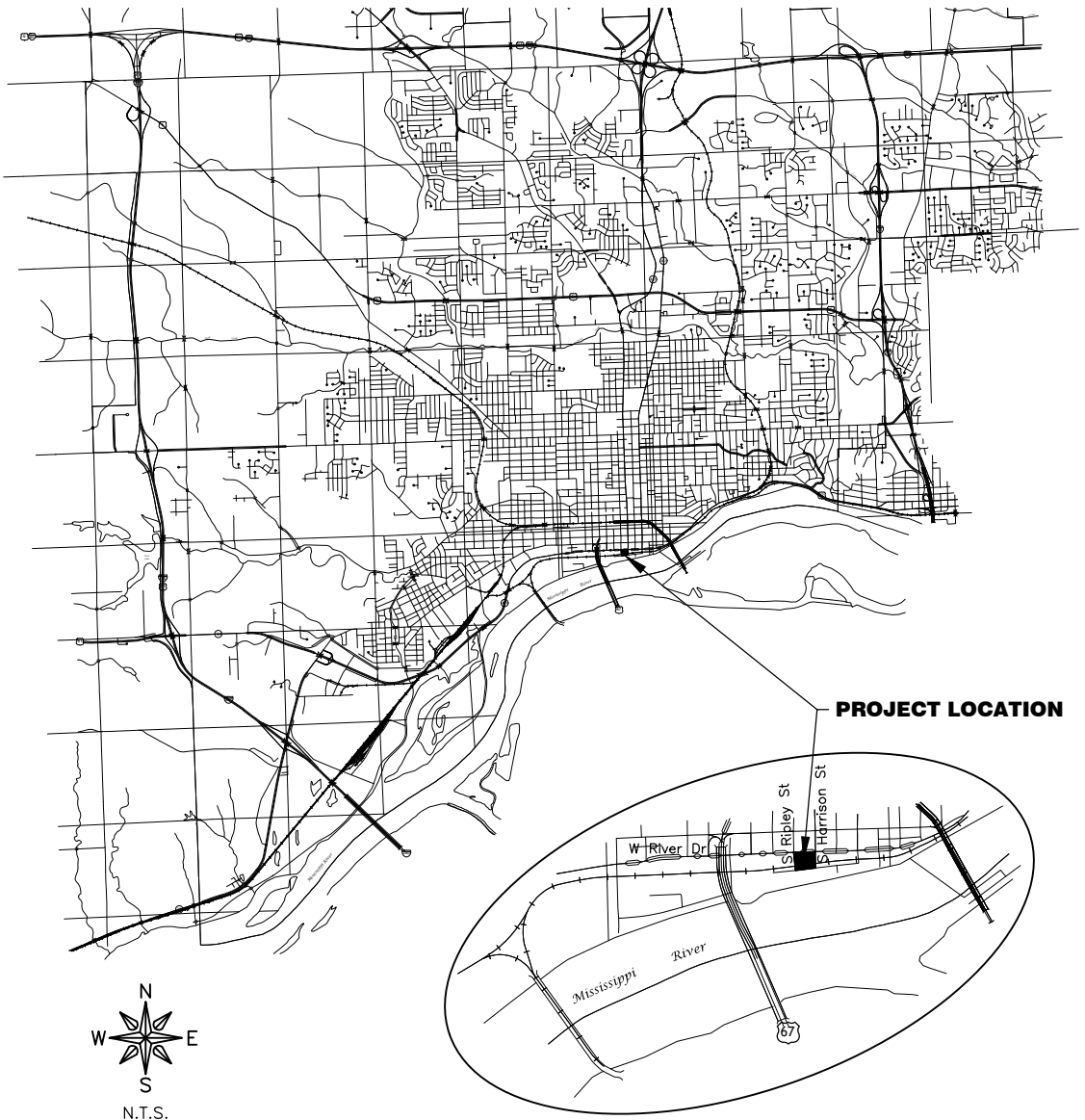
CITY OF DAVENPORT

THIS ENGINEERING DOCUMENT IS APPROVED

BRIAN SCHADT, P.E.
CIVIL ENGINEER

DATE

THE 2024 VERSION OF THE URBAN STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENTS, ALSO KNOWN AS SUDAS (2024), PLUS CITY OF DAVENPORT AND FEHR GRAHAM SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS SHALL APPLY TO CONSTRUCTION WORK ON THIS PROJECT.



LOCATION MAP



ILLINOIS IOWA WISCONSIN

CEDAR RAPIDS, IOWA
200 5TH AVENUE SE, SUITE 100
CEDAR RAPIDS, IA 52402
P# (319) 294-6909

PROFESSIONAL ENGINEER

DANIEL P.
CACERES
P28317

IOWA

I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Iowa.

_____ Date

Daniel P. Caceres, P.E.

License Number P28317

My license renewal date is December 31, 2026.

Pages or sheets covered by this seal: U-SHEETS

PROFESSIONAL ENGINEER

RYAN D.
HOSCH
24780

IOWA

I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Iowa.

_____ Date

Ryan D. Hosch, P.E.

License Number 24780

My license renewal date is December 31, 2027.

Pages or sheets covered by this seal: A, C, D, R, K SHEETS

ORIGINAL SET FOR PROJECT: 19-717PH05		DATE CREATED: 03/03/26
REVISIONS		
REV. NO.	DESCRIPTION	DATE

ABBREVIATIONS

<	ANGLE
ABC	AGGREGATE BASE COURSE
ACI	ACRE(S)
ACI	AMERICAN CONCRETE INSTITUTE
AGR	AGGREGATE
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION
ALT	ALTERNATE
ARCH	ARCHITECT
ASPH	ASPHALT
ASTM	AMERICAN SOCIETY OF TESTING AND MATERIALS
B	BALL VALVE
BFP	BACKFLOW PREVENTER
BIT	BITUMINOUS
BLDG	BUILDING
BLK	BLOCKING
BM	BENCHMARK
BOT	BOTTOM
BSMT	BASEMENT
BV	BUTTERFLY VALVE
B-B	BACK-TO-BACK OF CURB DIMENSION
CL or C	CENTERLINE
C TO C	CENTER TO CENTER
C & G	CURB AND GUTTER
CF	CUBIC FEET
CHD	CHORD LENGTH
CI	CAST IRON PIPE
CHK	CHECK VALVE
CLR	CLEAR
CMP	CORRUGATED METAL PIPE
CMU	CONCRETE MASONRY UNIT
CTY	COUNTY
CONC	CONCRETE
CONT	CONTINUOUS
C-B	CENTERLINE TO BACK OF CURB DIMENSION
COORD	COORDINATE
CU	COPPER PIPING
CTRS	CENTERS
CY	CUBIC YARDS
CS	CORPORATION STOP
D	DEGREE OF CURVE
DEP	DEPRESSED
DET	DETAIL
DIAG	DIAGONAL
DIM	DIMENSION
DI	DUCTILE IRON PIPE
DN	DOWN
DNSTR	DOWNSTREAM
DP	DRAINAGE PIPE/STORM PIPE
DWG	DRAWING
E	EAST
EJ	EXPANSION JOINT
EL, ELEV	ELEVATION
EP	EDGE OF PAVEMENT
EQUIP	EQUIPMENT
EQUIV	EQUIVALENT
EW	EACH WAY
EXP	EXPANSION
EX, EXIST	EXISTING
EXT	EXTERIOR
E =	EXTERNAL DISTANCE
FD	FLOOR DRAIN
FDN	FOUNDATION
FE	FIELD ENTRANCE
FF	FINISH FLOOR
FIL	FILLET
FIN	FINISH
FL	FLOW LINE
FLR	FLOOR
FM	FORCE MAIN
FND	FOUND
FRMG	FRAMING
FTG	FOOTING
F-F	FACE TO FACE
GA	GAUGE
GI	GALVANIZED IRON PIPE
GRD	GRADE
GRS	GRATING SUPPORT
GRT	GROUT
GV	GAS VALVE
GYP	GYPSUM
HSE	HOUSE
HC	HORIZONTAL CURVE
HMA	HOT MIX ASPHALT
HNGR	HANGER
HORIZ	HORIZONTAL
H.P.	HIGH POINT
HW	HOT WATER
HWH	HOT WATER HEATER
Δ =	CENTRAL ANGLE
I	MOMENT OF INERTIA
ID	INSIDE DIAMETER
INT	INTERIOR
INV	INVERT ELEVATION; BASED ON BENCH MARK DATUM
IP	IRON PIPE
JUST	JOIST
L	LENGTH OF CURVE
LAT	LATERAL
LAV	LAVATORY
LF	LINEAL FEET
L.P.	LOW POINT
LT	LEFT OF SURVEY BASE LINE
MAX	MAXIMUM
ME	MATCH EXISTING
MH	MANHOLE
MIN	MINIMUM
MJ	MECHANICAL JOINT
MTL	METAL
N	NORTH
No. or #	NUMBER
NOM	NOMINAL
NTS	NOT TO SCALE
OC	ON CENTER
OD	OUTSIDE DIAMETER
OO	OUTSIDE TO OUTSIDE
OPNG	OPENING
OPP	OPPOSITE
PC	POINT OF CURVATURE
PCC	PORTLAND CEMENT CONCRETE
PCF	POUNDS PER CUBIC FOOT
PDP	PERFORATED DRAIN PIPE

PE	POLYETHYLENE PIPE
PI	POINT OF INTERSECTION
PL	PLATE
PLG	PLUG VALVE
PLP	POLYPROPYLENE PIPE
PLYWD	PLYWOOD
PM	PRINCIPAL MERIDIAN
PR	PRESSURE REGULATORS
PRC	POINT OF REVERSE CURVATURE
PRESS	PRESSURE
PR, PROP	PROPOSED
PRV	PRESSURE REDUCING VALVE
PSF	POUNDS PER SQUARE FOOT
PSI	POUNDS PER SQUARE INCH
PSL	PIPE SLEEVE
PT	POINT OF TANGENCY
PLG	PLUG VALVE
PVC	POLYVINYL CHLORIDE (PLASTIC) PIPE
R	RADIUS
RDCR	REDUCER
RCCP	REINFORCED CONCRETE CYLINDER PIPE
RCP	REINFORCED CONCRETE PIPE
RD	ROOF DRAIN
REINF	REINFORCING
REQD	REQUIRED
ROW	RIGHT OF WAY
RFTN	RAFTER
RND	ROUND
RMR	RAILROAD
RRSP	RAILROAD SPIKE
RT	RIGHT
R&R	REMOVE AND REPLACE
S	SOUTH
SB	STREAM BED
SCHED	SCHEDULE
SEC	SECTION
SF	SQUARE FEET
SHR	SHOWER
SHT	SHEET
SHTG	SHEATHING
SP	SANITARY PIPE
SPA	SPACING OR SPACES
SPEC	SPECIFICATION
SQ	SQUARE
SS	SANITARY SERVICE
STA	STATION
STD	STANDARD
STL	STEEL
STRUCT	STRUCTURAL
SW	SIDEWALK
SY	SQUARE YARDS
SYM	SYMMETRICAL
TAN	TANGENT LENGTH
TBC	TOP BACK OF CURB
TBM	TEMPORARY BENCH MARK; BASED ON BENCHMARK DATUM
TD	TILE DRAIN
THK	THICK
TR	TREAD
TY	TYPE
TYP	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
UP	UTILITY POLE
UPSTR	UPSTREAM
UR	URINAL
USGS	US GEOLOGICAL SURVEY
VC	VERTICAL CURVE
VCP	VITRIFIED CLAY PIPE
VERT	VERTICAL
VOL	VOLUME
VPC	VERTICAL POINT OF CURVATURE
VPI	VERTICAL POINT OF INTERSECTION
VPRC	VERTICAL POINT OF REVERSE CURVATURE
VPT	VERTICAL POINT OF TANGENCY
W	WEST
WC	WATER CLOSET
WF	WIDE FLANGE
WM	WATER MAIN
WMQ	WATER MAIN QUALITY
WV	WATER VALVE
WGT	WEIGHT
WP	WEATHER PROOF
WS	WATER SERVICE
WWF	WELDED WIRE FABRIC
W	WITH
W/O	WITHOUT
XP	EXPLOSION PROOF

HATCH PATTERNS

	EARTH - FILL		BRICK
	EARTH - UNDISTURBED		STEEL
	ROCK (GEOLOGICAL)		INSULATION (LOOSE/ BATT)
	STONE OR RIP RAP		INSULATION (RIGID)
	GRAVEL		WOOD (ROUGH)
	CONCRETE		WOOD (BLOCKING)
	CONCRETE BLOCK		WOOD (FINISH)
	CMU		DETECTABLE WARNING
	ASPHALT PAVEMENT		

SYMBOLS

		EXISTING	PROPOSED			EXISTING	PROPOSED
CIVIL				WATER			
EXISTING	PROPOSED			EXISTING	PROPOSED		
EXISTING R.O.W.	PROPOSED R.O.W.			WATER SERVICE			
				WATER PIPE			
				FIRE HYDRANT			
				YARD HYDRANT			
				WATER VALVE WITH BOX			
				CURB STOP W/CURB BOX			
				REDUCER			
				WATER VALVE VAULT			
				11.25' BEND			
				22.50' BEND			
				45' BEND			
				90' BEND			
				TEE			
				CAP			
				WATER METER			
				SPRINKLER HEAD			
				TRACER WIRE BOX			
				STORM SEWER			
				EXISTING	PROPOSED		
				STORM SEWER			
				DRAIN TILE			
				DITCH LINE (PAVED)			
				DITCH LINE (UNPAVED)			
				STORM MANHOLE			
				CATCH BASIN			
				STORM SEWER INLET			
				STORM SEWER INLET - BEHIND CURB			
				DOWNSPOUT			
				CULVERT AND SIZE			
				RCCP OR RCP EQRS (RCAP) END SECTION			
				METAL OR HDPE END SECTION			
				FLOW DIRECTION			
				EROSION CONTROL			
				EROSION CONTROL BLANKET			
				TEMPORARY AND PERMANENT SEEDING AREA			
				UNDISTURBED AREA			
				STABILIZED CONSTRUCTION ENTRANCE			
				SILT FENCE			
				INLET PROTECTION			
				TEMPORARY SEDIMENT TRAP			
				CULVERT INLET PROTECTION			
				ROCK OUTLET PROTECTION			
				ROCK CHECK DAM - COURSE AGGREGATE			
				ROCK CHECK DAM - RIP RAP			
				DITCH CHECK			
MISC				UTILITY			
EXISTING	PROPOSED			EXISTING	PROPOSED		
S.B. #XX	S.B. #XX			FIBER OPTIC LINE			
MW #XX	MW #XX			UNDERGROUND TV CABLE			
				CABLE TV RISER PEDESTAL			
				OVERHEAD UTILITY			
				UNDERGROUND ELECTRIC			
SANITARY SEWER				ELECTRIC RISER PEDESTAL			
EXISTING	PROPOSED			ELECTRIC MANHOLE			
SAN	SAN			UNDERGROUND TELEPHONE			
SSW	SSW			TELEPHONE RISER PEDESTAL			
FM	FM			TELEPHONE MANHOLE			
CO	CO			UTILITY POLE			
				UTILITY POLE W/ METER			
				UTILITY POLE W/ TRANSFORMER			
				UTILITY POLE W/ LIGHT			
				UTILITY POLE WITH GUY WIRE AND ANCHOR			
				LIGHT (MAST MOUNTED)			
				LIGHT POLE (SINGLE FIXTURE)			
				YARD LIGHT			
				GAS MAIN			
				GAS METER			
				GAS VALVE			
				GAS STRUCTURE			
				TRAFFIC RELATED			
				EXISTING	PROPOSED		
				CONTROLLER			
				MAST ARM ASSEMBLY AND POLE			
				SIGNAL HEAD AND POST			
				SIGNAL HEAD			
				PEDESTRIAN HEAD			
				PEDESTRIAN PUSH-BUTTON			
				HAND HOLE			
				DOUBLE HAND HOLE			
				HAND HOLE OR JUNCTION BOX			
				HEAVY-DUTY HAND HOLE			
				EXISTING CONDUIT (LENGTH AND SIZE) PROP GALVANIZED STEEL OR PVC CONDUIT UPPER NUMERAL INDICATES LENGTH "T" INDICATES CONDUIT IN TRENCH "P" INDICATED CONDUIT PUSHED LOWER NUMERAL INDICATES SIZE AND TYPE			
				LUMINAIRE			
				ARROW - THROUGH, TURN LEFT			
				ARROW - THROUGH			
				ARROW - TURN LEFT			
				ARROW - TURN RIGHT			
				ONE DIRECTION TURN ONLY			
				HANDICAPPED PARKING STALL			
				TRAFFIC DETECTOR LOOP			
				TRAFFIC CONTROL BOX			

GENERAL NOTES

1. ALL WORK SHALL CONFORM TO AND BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE CODES AND ORDINANCES.
2. THE IOWA STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS), 2026 EDITION, PLUS CITY OF DAVENPORT SUPPLEMENTAL SPECIFICATIONS TO SUDAS, PLUS SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS AS PREPARED BY FEHR GRAHAM SHALL BE CONSIDERED A PART OF THESE DOCUMENTS AS IF BOUND HEREIN.
3. THE QUANTITIES INDICATED ON THE PROPOSAL FORM ARE APPROXIMATE ONLY, AND DO NOT CONSTITUTE A WARRANTY OR GUARANTEE BY THE JURISDICTION AS TO THE ACTUAL QUANTITIES INVOLVED IN THE WORK. SUCH QUANTITIES ARE TO BE USED FOR THE PURPOSE OF COMPARISON OF BIDS AND DETERMINING THE AMOUNT OF BID SECURITY, CONTRACT, AND PERFORMANCE, PAYMENT, AND MAINTENANCE BOND. IN THE EVENT OF DISCREPANCIES BETWEEN UNIT PRICES AND UNIT PRICE EXTENSIONS LISTED IN A BIDDER'S PROPOSAL, UNIT PRICES SHALL GOVERN AND UNIT PRICE EXTENSIONS SHALL BE CORRECTED, AS NECESSARY, FOR AGREEMENT WITH UNIT PRICES. THE JURISDICTION EXPRESSLY RESERVES THE RIGHT TO INCREASE OR DECREASE THE QUANTITIES DURING CONSTRUCTION, AND TO MAKE REASONABLE CHANGES IN DESIGN, PROVIDED SUCH CHANGES DO NOT MATERIALLY CHANGE THE INTENT OF THE CONTRACT. THE AMOUNT OF WORK TO BE PAID FOR SHALL BE BASED UPON THE ACTUAL QUANTITIES PERFORMED.
4. CONSTRUCTION SURVEY FOR THIS PROJECT TO BE PROVIDED BY THE CONTRACTOR.
5. THE CONTRACTOR SHALL NOTIFY ALL APPROPRIATE ENGINEERING DEPARTMENTS AND UTILITY COMPANIES PRIOR TO CONSTRUCTION. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO AVOID DAMAGE TO ANY EXISTING UTILITY. IOWA CODE 480, UNDERGROUND FACILITIES INFORMATION, REQUIRES NOTICE TO IOWA ONE CALL (1-800-292-8989) NOT LESS THAN 48 HOURS BEFORE EXCAVATION, EXCLUDING WEEKENDS AND LEGAL HOLIDAYS.
6. THE LOCATION OF EXISTING UNDERGROUND UTILITIES AND ROCK ELEVATIONS ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES AND ROCK ELEVATIONS BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES AND ROCK ELEVATIONS.
7. THE CONTRACTOR SHALL VISIT THE SITE AND INSPECT THE PROJECT AREA AND BECOME THOROUGHLY FAMILIAR WITH THE ACTUAL JOB CONDITIONS PRIOR TO BIDDING AND THE START OF ANY WORK. FAILURE TO VISIT THE SITE SHALL NOT RELIEVE THE CONTRACTOR FROM PERFORMING THE WORK IN ACCORDANCE WITH THESE DRAWINGS.
8. THE CONTRACTOR SHALL VERIFY AT THE SITE, ALL DIMENSIONS AND CONDITIONS SHOWN ON THE DRAWINGS, AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES, OMISSIONS, AND/OR CONFLICT PRIOR TO PROCEEDING WITH THE WORK.
9. THE CONTRACTOR SHALL NOT SCALE DRAWINGS. DIMENSIONS SHALL GOVERN. LARGE SCALE DRAWINGS SHALL GOVERN OVER SMALL SCALE DRAWINGS. NOTES AND DETAILS ON THE DRAWINGS SHALL APPLY TO ALL SIMILAR CONDITIONS WHETHER THEY ARE REPEATED OR NOT.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES OUTSIDE THE CONSTRUCTION LIMITS RESULTING FROM NEGLIGENCE.
11. CONTRACTOR SHALL PROTECT EXISTING FACILITIES, BUILDINGS, AND OTHER APPURTENANCES NOT TO BE REMOVED FROM THE SITE DURING THE CONSTRUCTION ACTIVITIES.
12. CONTRACTOR SHALL CONFINE HIS WORK TO THE CONSTRUCTION LIMITS AND EASEMENTS. IF THE CONTRACTOR OBTAINS ADDITIONAL EASEMENT FOR THE STORAGE OF EQUIPMENT AND MATERIALS, COPIES OF THE AGREEMENTS WITH THE PROPERTY OWNERS SHALL BE PROVIDED TO THE OWNER.
13. CONTRACTOR SHALL SUBMIT A DETAILED CONSTRUCTION SCHEDULE AND STAGING PLAN A MINIMUM OF TWO (2) DAYS PRIOR TO THE PRECONSTRUCTION MEETING.
14. CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN ACCESS TO INDIVIDUAL PROPERTIES DURING CONSTRUCTION WHENEVER PRACTICAL. CONTRACTOR SHALL NOTIFY RESIDENTS OF ACCESS RESTRICTIONS MINIMUM OF 24 HOURS PRIOR TO REMOVAL OF EXISTING ACCESS.
15. CONTRACTOR SHALL SUBMIT FOR ACCEPTANCE WORK PLANS AND SCHEDULES FOR ACCOMPLISHMENT OF TEMPORARY AND PERMANENT EROSION CONTROL PRIOR TO THE START OF CONSTRUCTION.
16. CONTRACTOR SHALL COORDINATE TEMPORARY DISRUPTION OF UTILITY SERVICES WITH THE CITY OF DAVENPORT, AFFECTED UTILITY COMPANIES AND/OR AFFECTED PROPERTY OWNERS WHEN RELOCATING EXISTING FACILITIES, CONNECTING TO EXISTING FACILITIES AND PLACING NEW SERVICES.
17. ALL CITY SIGNS IN THE PROJECT BOUNDARIES AFFECTED BY CONSTRUCTION OPERATIONS SHALL BE REMOVED, SALVAGED, AND PROPERLY REINSTALLED BY THE CONTRACTOR WITH PERFORATED SQUARE STEEL POSTS. ANY DAMAGED, LOST OR STOLEN SIGNS OR SIGNS THAT HAVE NOT BEEN REINSTALLED SHALL BE AT THE COST OF THE CONTRACTOR. SIGNS DEEMED UNNECESSARY FOR CURRENT USES SHALL BE RETURNED TO THE PUBLIC WORKS YARD AT 1200 E. 46TH STREET.
18. CONTRACTOR TO COMPLY WITH ALL CANADIAN PACIFIC RAILWAY REQUIREMENTS FOR WORK ALONG THE RAILROAD RIGHT-OF-WAY. COSTS FOR ALL REQUIREMENTS INCLUDING ANY FLAGGING AND RAILROAD INSURANCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
19. CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF CANADIAN PACIFIC RAILROAD STANDARD PROVISIONS.

NO.	CODE	DESCRIPTION	QUANTITY	UNIT
1	2010-C	CLEARING AND GRUBBING	1	LS
2	2010-D-1	TOPSOIL, ON-SITE	90	CY
3	2010-D-3	TOPSOIL, OFF-SITE	143	CY
4	2010-L-1	COMPACTION TESTING	1	LS
5	2010-E	EXCAVATION, CLASS 10	400	CY
6	2010-G	SUBGRADE PREPARATION	1267	SY
7	2010-J	SUBBASE, MODIFIED, 6-INCH	1267	SY
8	4020-A-1	4040-A SUBDRAIN, PE, 6-INCH	59	LF
9	4020-A-1	STORM SEWER, TRENCHED, HDPE, 12-INCH	276	LF
10	4020-I	INLINE RUBBER CHECK VALVE	1	EA
11	6010-B	AREA INLET, SW-511	6	EA
12	6010-E	MANHOLE ADJUSTMENT, MINOR	3	EA
13	7010-A	PAVEMENT, PCC, 6 INCH	1267	SY
14	7030-A-1	REMOVAL OF SIDEWALK	129	SY
15	7030-G	DETECTABLE WARNING	10	SF
16	7040-H	PAVEMENT REMOVAL	142	SY
17	7040-I	BRICK BARRIER CURB REMOVAL	575	LF
18	7100-D	BRICK PAVING REMOVAL	1111	SY
19	8030-A	TEMPORARY TRAFFIC CONTROL	1	LS
20	8040-B	TRAFFIC SIGNS	2	EA
21	9010-A	CONVENTIONAL SEEDING, FERTILIZING, AND MULCHING, LOW MOW FESCUE (LAWN)	0.04	AC
22	9020-A	CONVENTIONAL SEEDING, FERTILIZING, AND MULCHING, NATIVE	0.09	AC
23	9020-A-2	SWPPP MANAGEMENT	1	LS
24	9030-D	PLANTS WITH WARRANTY	1	LS
25	9040-T-1	INLET PROTECTION DEVICE	7	EA
26	9040-T-2	INLET PROTECTION DEVICE, MAINTENANCE	7	EA
27	11010-A	CONSTRUCTION SURVEY	1	LS
28	11020-A	MOBILIZATION	1	LS
29	11050-A	CONCRETE WASHOUT	1	LS
30	12010-999A	EARTHEN BERM	500	CY
31	12020-999A	BRICK FACED FREESTANDING FLOODWALL	67	LF
32	12030-999A	SHEETPILE FLOOD WALL	5760	SF
33	12040-999A	FLAGGERS	10	Days
34	12050-999A	RAILROAD INSURANCE	1	LS
35	12060-999A	REMOVABLE FLOOD GATE	1	LS
36	12070-999A	CONSTRUCTION CONTINGENCY	1	LS



ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
CITY OF DAVENPORT
226 WEST 4TH STREET
DAVENPORT, IA 52801

PROJECT AND LOCATION:
UNION STATION FLOOD REPAIRS
DAVENPORT, IOWA

DRAWN BY: JMP
APPROVED BY: RH
DATE: 03/03/26
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
GENERAL NOTES AND ESTIMATED QUANTITIES

SET TYPE: PRELIMINARY
\\rockford\Drawings\C30\19-717 Davenport Flood Repairs\Plans\19-717 PH05 Plans.dwg, C.01

JOB NUMBER:
19-717PH05

SHEET NUMBER:
C.01

THE FOLLOWING SUPPLEMENTAL SPECIFICATIONS ARE IN ADDITION TO THE CITY OF DAVENPORT, IOWA, SUPPLEMENTAL SPECIFICATIONS TO SUDAS.

DIVISION 1 – GENERAL PROVISIONS AND COVENANTS

SECTION 1010
1.03 – DEFINITIONS AND TERMS
JURISDICTION: THE JURISDICTION IS THE CITY OF DAVENPORT. ANY REFERENCE TO EITHER SHALL BE CONSIDERED ONE IN THE SAME.

SECTION 1020
1.14 – OPENING OF PROPOSALS
ONLY BID TOTALS WILL BE MADE PUBLICLY AVAILABLE AT AND IMMEDIATELY AFTER THE BID OPENING. AN ITEMIZED BID TABULATION WILL BE MADE PUBLICLY AVAILABLE FOR INTERESTED PARTIES AFTER THE PROJECT HAS BEEN AWARDED BY THE OWNER.

SECTION 1030
1.02 – RELEASE OF BID SECURITY
A. THE JURISDICTION SHALL RETAIN THE BID SECURITY OF THE LOWEST THREE BIDDERS. THE BID SECURITIES OF THE THREE LOWEST BIDDERS WILL BE RELEASED AFTER THE JURISDICTION'S APPROVAL OF THE CONTRACT EXECUTED BY THE LOWEST RESPONSIVE, RESPONSIBLE BIDDER.

SECTION 1040
1.05 – PLANS
SUDAS STANDARD SPECIFICATIONS APPLY. IN ADDITION TO SECTION 1040–1.05 THE FOLLOWING APPLY.
A. CONTRACTOR'S BIDS SHALL BE BASED ON THE FINAL PLANS AND ANY ADDENDUM RECEIVED.
B. NO CAD FILES OR ELECTRONIC SURFACE INFORMATION WILL BE AVAILABLE FOR BIDDING PURPOSES.
C. NO CAD FILES OR ELECTRONIC SURFACE INFORMATION WILL BE AVAILABLE TO THE CONTRACTOR FOLLOWING AWARD OF THE PROJECT.

1.06 – INCREASE OR DECREASE OF WORK
B. QUANTITY CHANGE, REGARDLESS OF THE PERCENTAGE INCREASE OR DECREASE OF THE TOTAL BID, SHALL NOT AFFECT THE UNIT BID PRICE OF THAT ITEM.

SECTION 1050
1.03 – COOPERATION BY THE CONTRACTOR – SPECIAL ATTENTION TO THIS SECTION IS REQUESTED OF ALL BIDDING CONTRACTORS
1.12 – SALVAGE

A. CONTRACTOR SHALL SALVAGE BRICK PAVERS FOR THE JURISDICTION. CONTRACTOR IS RESPONSIBLE FOR HAULING TO THE DESIGNATED STOCKPILE AT THE PUBLIC WORKS MARQUETTE YARD AT 232 SOUTH MARQUETTE STREET. CONTRACTOR SHALL CHECK IN WITH THE PERSON AT THE ASPHALT PLANT OFFICE (VISIBLE FROM THE YARD ENTRANCE GATE) PRIOR TO DELIVERY. BRICKS THAT ARE OVERLAID WITH ASPHALT OR SEAL COAT SHALL NOT BE SALVAGED AND SHALL BE THE PROPERTY OF THE CONTRACTOR AND DISPOSED OF PROPERLY. SPECIAL CARE SHALL BE TAKEN TO ENSURE THE SALVAGED BRICKS ARE RELATIVELY CLEAN OF SOIL BEFORE DEPOSITED IN THE STOCKPILES. CONTRACTOR IS NOT RESPONSIBLE FOR PALLETIZING BRICKS. CONTRACTOR IS STRONGLY ENCOURAGED TO DELIVER BRICKS IMMEDIATELY UPON REMOVAL TO PREVENT THEFT.

SECTION 1060
1.04 – STORAGE OF MATERIALS
THE BRICK PAVEMENT AREA ON THE SOUTH SIDE OF THE PROJECT AND NORTH OF THE RAILROAD RIGHT-OF-WAY MAY BE USED AS A STAGING AREA FOR STORAGE OF EQUIPMENT AND MATERIALS FOR THIS PROJECT WHERE INDICATED ON THE PLANS. CONTRACTOR SHALL RETURN AREA TO A CONDITION SIMILAR TO THAT PRIOR TO CONSTRUCTION. THIS INCLUDES, BUT IS NOT LIMITED TO: SMOOTHING ANY RUTS, REMOVING ALL TRASH AND DEBRIS, AND REPAIRING OR RESEEDING THE AREA. NO PAYMENT WILL BE MADE FOR WORK OR MATERIALS REQUIRED TO RETURN STAGING AREA TO PRECONSTRUCTION CONDITION.

SECTION 1070
2.02 – CONVENIENCE AND SAFETY – SPECIAL ATTENTION TO THIS SECTION IS REQUESTED OF ALL BIDDING CONTRACTORS
C. WORK SHALL BE COMPLETED IN A MANNER THAT WILL CAUSE THE LEAST INCONVENIENCE AND ANNOYANCE TO THE PUBLIC AND PROPERTY OWNERS ABUTTING THE WORK AREA, AND SHALL PROVIDE ACCESS TO THE ABUTTING PROPERTY TO THE GREATEST EXTENT PRACTICABLE. CONTRACTOR SHALL NOTIFY PROPERTY OWNERS A MINIMUM OF 48 HOURS IN ADVANCE WHEN ACCESS WILL BE RESTRICTED TO THEIR PROPERTIES.

2.05 – EXPLOSIVES
A. USE: DUE TO THE RISK OF COLLATERAL DAMAGE, BLASTING WILL NOT BE ALLOWED FOR THIS PROJECT AS PART OF ROCK REMOVAL ACTIVITIES.

2.13 – BORROW AND WASTE SITES
A. CONTRACTOR SHALL SECURE AND OPERATE, AT ITS OWN EXPENSE, SITES FOR DISPOSAL OF CLASS 12 ROCK EXCAVATION, STRUCTURES, SURFACING MATERIALS, AND RUBBISH AND DEBRIS.

SECTION 1080
1.02 – CONTRACT TIME
A. CONTRACTOR SHALL FULLY COMPLETE THE BASE PROJECT IN 40 WORKING DAYS. LIQUIDATED DAMAGES ARE \$1,000/DAY.

SECTION 1090
1.05 – PROGRESS PAYMENTS
D. CITY WILL PROVIDE PROGRESS PAYMENTS FOR CONTRACTOR REVIEW/APPROVAL EVERY 4 WEEKS UNLESS OTHERWISE AGREED BY THE CONTRACTOR AND ENGINEER.

DIVISION 2 – EARTHWORK

SECTION 2010
1.03 – SUBMITTALS
A. THE JURISDICTIONAL ENGINEER HEREBY REQUESTS THAT ALL MATERIALS TO BE INCORPORATED INTO THE WORK HAVE CERTIFICATIONS FURNISHED WHICH SHOW THAT THE MATERIALS COMPLY WITH SPECIFICATIONS PRIOR TO ANY CONSTRUCTION.

DIVISION 3 – TRENCH AND BACKFILL

SECTION 3010
3.05 – PIPE BEDDING AND BACKFILL
PIPE EMBEDMENT REQUIREMENTS FOR FLEXIBLE GRAVITY PIPE:
HDPE DUAL WALL GRAVITY PIPE: CLASS F–3

DIVISION 4 – SEWERS AND DRAINS

SECTION 4020
1.03 – SUBMITTALS
A. THE JURISDICTIONAL ENGINEER HEREBY REQUESTS THAT ALL MATERIALS TO BE INCORPORATED INTO THE WORK HAVE CERTIFICATIONS FURNISHED WHICH SHOW THAT THE MATERIALS COMPLY WITH SPECIFICATIONS PRIOR TO ANY CONSTRUCTION.

1.08 – MEASUREMENT AND PAYMENT
A. 1. A. MEASURE EACH SIZE AND TYPE OF PIPE INSTALLED FROM INSIDE WALL OF INTAKE/MANHOLE TO INSIDE WALL OF INTAKE/MANHOLE.

I. INLINE RUBBER CHECK VALVE:
1. MEASUREMENT: EACH VALVE WILL BE COUNTED.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE FOR EACH VALVE INSTALLED.
3. INCLUDES: UNIT PRICE INCLUDES, BUT IS NOT LIMITED TO, FURNISHING AND INSTALLING INTERNAL CLAMPS AND HARDWARE REQUIRED TO INSTALL THE VALVE PER MANUFACTURER'S RECOMMENDATIONS.

2.07 – INLINE RUBBER CHECK VALVE
ALL RUBBER, PLY REINFORCED, VULCANIZED UNIBODY CONSTRUCTION CHECK VALVE WITH SLIP-IN CUFF CONNECTION, SUCH AS CHECKMATE[®] ULTRAFLEX[™] INLINE CHECK VALVE BY RED VALVE[®] OR PREAPPROVED EQUIVALENT.

3.09 – STORM SEWER ABANDONMENT
A. 1. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE SEWER LINE IS NOT IN USE. VERIFICATION IS INCIDENTAL TO THE PLUG/CAP
B. 1. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE SEWER LINE IS NOT IN USE. VERIFICATION IS INCIDENTAL TO THE FILLING OF THE SEWER.

3.12 – INLINE RUBBER CHECK VALVE
INSTALL PER MANUFACTURER'S RECOMMENDATIONS.

DIVISION 6 – STRUCTURES FOR SANITARY AND STORM

SECTION 6010
1.03 – SUBMITTALS
THE JURISDICTIONAL ENGINEER HEREBY REQUESTS THAT ALL MATERIALS TO BE INCORPORATED INTO THE WORK HAVE CERTIFICATIONS FURNISHED WHICH SHOW THAT THE MATERIALS COMPLY WITH SPECIFICATIONS PRIOR TO ANY CONSTRUCTION.

DIVISION 7 – STREETS & RELATED WORK

SECTION 7010
1.03 – SUBMITTALS
ALL SUBMITTALS LISTED IN THE STANDARD SPECIFICATIONS SHALL BE PROVIDED.

1.07 – SPECIAL REQUIREMENTS
A. MATURITY METHOD FOR DETERMINATION OF OPENING TIME MAY BE USED FOR THIS PROJECT BUT THERE WILL BE NO ADDITIONAL PAYMENT. IF USED, CONTRACTOR/SUPPLIER SHALL DEVELOP AND SUBMIT MATURITY CURVE PRIOR TO THE START OF PAVING OPERATIONS.

2.01 – MATERIALS
D. 1. CLASS 3 DURABILITY OR BETTER ONLY FOR THIS PROJECT.
2.02 – CONCRETE MIXES
A. 1. MINIMUM COMPRESSIVE STRENGTH SHALL BE 4,500 PSI AT 28 DAYS. CONTRACTOR MAY USE IOWA DOT CLASS M MIX MEETING THE REQUIREMENTS OF MATERIALS I.M. 529, BUT NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS MIX. CONTRACTOR MAY USE C–SUD OR CV–SUD MIXES, BUT NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS MIX.

3.02 – PAVEMENT CONSTRUCTION
I. 5. SURFACE TREATMENT SHALL CONSIST OF A SOFT BROOM FINISH TO PROVIDE A NON–SKID SURFACE.
L. JOINT SEALING IS NOT REQUIRED.

SECTION 7030
2.01 – PORTLAND CEMENT CONCRETE
A. CLASS C, 4,500 PSI AT 28 DAYS COMPRESSIVE STRENGTH REQUIRED. CLASS 3 DURABILITY OR BETTER ONLY FOR THIS PROJECT.

3.04 – PCC RECREATIONAL TRAILS, SIDEWALKS, AND DRIVEWAYS
F. 2. B. 3. ALL TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED. JOINTS MAY BE SAWED WITHIN 12 HOURS OF PLACEMENT WITH A 1/8 INCH BLADE SAW TO A DEPTH OF 1/3 THE PAVEMENT THICKNESS. USE A STRAIGHTEDGE IF JOINTS ARE SAWED WITH A HAND–HELD SAW.
3. B. 2. ALL LONGITUDINAL CONTRACTION JOINTS SHALL BE SAWED. JOINTS MAY BE SAWED WITH A 1/8 INCH BLADE TO A DEPTH OF 1/3 THE PAVEMENT THICKNESS. USE A STRAIGHTEDGE IF JOINTS ARE SAWED WITH A HAND–HELD SAW.

SECTION 7100
1.08 – MEASUREMENT AND PAYMENT
D. BRICK PAVING REMOVAL
1. MEASUREMENT: MEASUREMENT WILL BE IN SQUARE YARDS FOR THE AREA OF BRICK PAVEMENT REMOVED. THE AREA OF MANHOLES, INTAKES OR OTHER FIXTURES IN PAVEMENT WILL NOT BE DEDUCTED FROM THE MEASURED AREA.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE PER SQUARE YARD OF REMOVED BRICKS.
3. INCLUDES: UNIT PRICE INCLUDES, BUT IS NOT LIMITED TO, REMOVING, HAULING AND DISPOSAL OF UNNEEDED MATERIALS, SUCH AS HMA OR PCC PATCHES, CLEANING SALVAGED BRICKS FROM BOTH PROJECT SITE AND JURISDICTION'S STOCKPILE, TRANSPORTING BRICKS TO THE JURISDICTIONS'S STOCKPILE AND PROTECTING BRICKS FROM THEFT OR DAMAGE. PURINGTON PAVERS SHALL BE DELIVERED TO THE CITY MARQUETTE YARD.

DIVISION 8 – TRAFFIC CONTROL

SECTION 8040
1.01 – SECTION INCLUDES
F. ROADWAY SIGNAGE INCLUDING EXISTING AND PERMANENT. SIGNAGE FOR TRAFFIC CONTROL IS NOT INCLUDED IN THIS SECTION.
1.02 – DESCRIPTION OF WORK
A. TRAFFIC SIGN MATERIALS, INSTALLATION, MEASUREMENT, AND PAYMENT SHALL BE COMPLETED PER SECTION 2524 OF THE 2015 EDITION OF THE IOWA DOT STANDARD SPECIFICATIONS WITH GS–15010 REVISIONS AND THE BELOW SUPPLEMENTAL SPECIFICATIONS.

B. NEW SIGNS SHALL BE COMPLIANT WITH THE CURRENT MUTCD AND BE OF THE TYPE AND SIZE AS NOTED IN THE PLANS.
1.08 – MEASUREMENT AND PAYMENT
A. ALUMINUM SHEET SIGN:

1. MEASUREMENT: EACH SIGN WILL BE COUNTED.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE FOR EACH SIGN INSTALLED.
3. INCLUDES: UNIT PRICE INCLUDES, BUT IS NOT LIMITED TO, FURNISHING AND INSTALLING SIGNS; MOUNTING HARDWARE; REMOVAL AND DISPOSAL OF OLD SIGNS.

2.01 – GENERAL
SIGNS SHALL BE ALUMINUM INDICATING VAN ACCESSIBLE HANDICAP PARKING WITH STANDARD BLUE AND WHITE COLOR SCHEME. POST SHALL BE PERFORATED SQUARE STEEL POSTS.

3.01 – INSTALLATION
EXISTING SIGNS SHALL BE REMOVED AND SALVAGED TO PUBLIC WORKS YARD AT 1200 E. 46TH STREET. NEW HANDICAP PARKING SIGNS SHALL BE MOUNTED ON PERFORATED SQUARE STEEL POSTS. SIGNS SHALL BE FASTENED WITH STAINLESS STEEL STRAPS AND RUST RESISTANT BOLTS AND NUTS.

DIVISION 9 – SITE WORK & LANDSCAPING

SECTION 9010
1.02 – DESCRIPTION OF WORK
COMPLETED INSTALLATION SHALL INCLUDE PREPARATION OF THE SEEDBED, FURNISHING AND INSTALLING SEED, FERTILIZER AND MULCH, MAINTENANCE, AND GUARANTEE FOR COMPLETED SEEDED AREAS.

1.07 – SPECIAL REQUIREMENTS
A. WARRANTY IS REQUIRED AND IS INCIDENTAL TO THE SEEDING BID ITEM. NO SEPARATE BID ITEM IS ALLOWED. WARRANTY IS FOR ONLY PERMANENT SEEDING WITHIN THE DATES FOR EACH VARIETY SPECIFIED PER SECTION 9010, 2.02.
B. WARRANTY PERIOD IS TWO FULL YEARS FROM ACCEPTANCE.

1.08 – MEASUREMENT AND PAYMENT
A. 1. SEEDING FOR A COMPLETED INSTALLATION SHALL BE MEASURED IN ACRES, OF ACCEPTED SEEDING WITHIN THE CONTRACT OR EASEMENT LIMITS. SEEDING ITEM INCLUDES SUPPLYING AND APPLYING PROPER SEED, FERTILIZER, AND MULCH. DIFFERENT PAYMENT FOR METHOD OF APPLICATION WILL NOT BE ALLOWED. AT THE CONTRACTOR'S OPTION, HYDRAULIC OR PNEUMATIC SEEDING MAY BE USED, BUT WILL NOT BE PAID SEPARATELY.

4. FERTILIZING SHALL BE INCIDENTAL TO THE SEEDING BID ITEM AND WILL NOT BE PAID SEPARATELY.
5. MULCHING SHALL BE INCIDENTAL TO THE SEEDING BID ITEM AND WILL NOT BE PAID SEPARATELY.
E. WARRANTY FOR SEEDING, FERTILIZING, AND MULCHING IS REQUIRED BUT IS INCIDENTAL TO THE SEEDING BID ITEM. WARRANTY PERIOD SHALL BE FOR TWO FULL YEARS FROM THE DATE OF ACCEPTANCE. NO SEPARATE BID ITEM IS ALLOWED.

2.02 – SEED MIXTURES AND SEEDING DATES
ALL SEEDING SHALL BE TYPE 1 (PERMANENT LAWN MIXTURE). ALL TEMPORARY SEEDING SHALL BE TYPE 4 (URBAN TEMPORARY EROSION CONTROL MIXTURE). DELETE OATS IN TYPE 4 AS ALL AREAS ARE PREVIOUSLY ESTABLISHED LAWNS.

3.08 – RE–SEEDING
A. WHEN ALL WORK RELATED TO SEEDING ON AN AREA HAS BEEN COMPLETED BUT IS WASHED OUT OR DAMAGED PRIOR TO FINAL ACCEPTANCE OF THE SEEDING AREA, THE AREA SHALL BE RESEDED, REFERTILIZED, AND REMULCHED WITHOUT ADDITIONAL COMPENSATION.

3.10 – ACCEPTANCE AND WARRANTY
B. 1. REQUIRED BUT INCIDENTAL TO THE SEEDING BID ITEM AND SHALL NOT BE PAID SEPARATELY.

2. THE WARRANTY PERIOD IS TWENTY–FOUR MONTHS BEGINNING ON THE DATE OF ACCEPTANCE.

SUDAS DIVISION 12.000 – SPECIAL PROVISIONS

SECTION 12,010 – EARTHEN BERM

1.08 – MEASUREMENT AND PAYMENT
A. EARTHEN BERM:
1. MEASUREMENT: MEASUREMENT WILL BE THE PLAN QUANTITY IN CUBIC YARDS, WITHOUT FINAL FIELD MEASUREMENT. ADJUSTMENTS MAY BE MADE TO THE PLAN QUANTITIES IF AGREED TO BY THE OWNER, THE ENGINEER AND THE CONTRACTOR.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE PER CUBIC YARD
3. INCLUDES: WORK INCLUDES, BUT IS NOT LIMITED TO, SITE PREPARATION FOR AND THE CONSTRUCTION OF THE EMBANKMENT, EXCAVATION AND REMOVAL OF NATIVE MATERIALS TO PLAN GRADE, OVERHAUL, PROVIDE AND PLACE PROPER MATERIALS, PROPER SLOPING AND SHAPING OF BERM, AND PROPER COMPACTION OF MATERIALS PER SPECIFICATIONS.

2.01 – SUITABLE EMBANKMENT MATERIALS
A. THE CONTRACTOR SHALL BACKFILL AND BUILD THE LEVEE WITH IMPORTED MATERIAL HAVING:
1. NO LESS THAN 35% OF THE PARTICLES BY WEIGHT PASSING A #200 SIEVE.
2. A LIQUID LIMIT (LL) NO HIGHER THAN 50 ON THAT PORTION OF THE MATERIAL PASSING A #40 SIEVE.
3. A PLASTIC INDEX (PI) OF NO LESS THAN 12 ON THAT PORTION OF THE MATERIAL PASSING A #40 SIEVE.
4. A LIQUID LIMIT (LL) AND PLASTIC INDEX (PI) PLOT ABOVE THE "A" LINE ON THE PLASTICITY CHART.
5. MATERIAL CLASSIFYING AS CLAYEY SAND (SC) ACCORDING TO THE UNIFIED SOIL CLASSIFICATION SYSTEM (USCS), OR AS SANDY LEAN CLAY, LEAN CLAY WITH SAND, OR LEAN CLAY (CL), WOULD MEET THESE CRITERIA.
B. CONTRACTOR SHALL PROVIDE CERTIFIED SOIL CLASSIFICATION OF PROVIDED MATERIAL FROM A LICENSED GEOLOGIST OR GEOTECHNICAL ENGINEER.

3.01 – EXECUTION
A. REMOVE ALL GROUND COVER AND TOPSOIL FROM THE BERM AREA.
B. EXCAVATE NATIVE MATERIAL TO REQUIRED GRADE. REMOVE AND DISPOSE OF PROPERLY.
C. PRIOR TO COMPACTION THE CONTRACTOR SHALL MOISTEN OR DRY BACKFILL TO MOISTURE CONTENTS WITHIN ONE PERCENTAGE POINT BELOW TO THREE PERCENTAGE POINTS ABOVE THEIR OPTIMUM MOISTURE CONTENTS.
D. THE CONTRACTOR SHALL PLACE BACKFILL IN LOOSE LIFTS 6 TO 12 INCHES THICK PRIOR TO COMPACTION (WITH THE LESSER BEING MORE APPROPRIATE FOR FINER–GRAINED HIGHER PLASTICITY SOILS)
e.REFER TO SUDAS SECTION 2010. 3.04 – EMBANKMENT CONSTRUCTION. USE TYPE A COMPACTION.
b. ADJUSTMENTS MAY BE REQUIRED BASED ON INITIAL COMPACTION TEST RESULTS.
E. THE CONTRACTOR SHALL COMPACT TO AT LEAST 95% OF THE MATERIAL'S MAXIMUM STANDARD PROCTOR DRY DENSITY. COMPACTION TESTING IS PAID UNDER THAT ITEM.

SECTION 12,020 – BRICK FACED FREESTANDING FLOOD WALL

1.08 – MEASUREMENT AND PAYMENT
A. BRICK FACED FREESTANDING FLOOD WALL:
1. MEASUREMENT: MEASUREMENT WILL BE PER LINEAR FOOT AS MEASURED ALONG THE TOP CENTER OF THE WALL.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE PER LINEAR FOOT.
3. INCLUDES: WORK INCLUDES, BUT IS NOT LIMITED TO, SITE PREPARATION FOR AND THE CONSTRUCTION OF THE WALL, FOUNDATION, REINFORCEMENT, DRAINAGE TILE AND GRANULAR BACKFILL, BRICK FAÇADE, AND ALL MATERIALS AND LABOR FOR A COMPLETE INSTALLATION PER PLAN. NOTE THAT BRICK FAÇADE IS NOT REQUIRED ON ALL PARTS OF ALL WALLS. SEE PLANS FOR DETAILS.
4.

2.01 – BRICK FACED FLOOD WALL
A. REFER TO SUDAS SECTION 6010, 2.03 – CAST–IN–PLACE FOR CONCRETE WALL.
B. BRICK SHALL BE STANDARD MASONRY UNITS WITH GROUT TO MATCH THE BUILDINGS ON THE PROJECT PER OWNER APPROVALS.

3.01 – EXECUTION
A. REFER TO SUDAS SECTION 6010, 3.02 FOR CONCRETE WALL.

SECTION 12,030 – SHEETPILE FLOOD WALL

1.08 – MEASUREMENT AND PAYMENT
1. MEASUREMENT: MEASUREMENT WILL BE THE PLAN QUANTITY PER SQUARE FOOT, WITHOUT FINAL FIELD MEASUREMENT. ADJUSTMENTS MAY BE MADE TO THE PLAN QUANTITIES IF AGREED TO BY THE OWNER, THE ENGINEER, AND THE CONTRACTOR.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE PER SQUARE FOOT.
3. INCLUDES: WORK INCLUDES, BUT IS NOT LIMITED TO, SITE PREPARATION FOR AND CONSTRUCTION OF THE WALL, SHEETPIILING, REINFORCEMENT, FORMING, SLIP LINER INSTALLATION, COLORED CONCRETE, FINISHING.

3.01 – EXECUTION
A. CONTRACTOR SHALL SUBMIT SAMPLES OF CONCRETE COLOR. COLOR SHALL BE LIGHT BROWN/LIMESTONE.
B. CONTRACTOR SHALL SUBMIT SAMPLES OR DATA SHEETS FOR SLIP LINER. SEE DETAILS SHEETS FOR PATTERN OF WALL.

SECTION 12,040 – FLAGGERS

1.08 – MEASUREMENT AND PAYMENT
1. MEASUREMENT: MEASUREMENT WILL BE FOR 10 DAYS.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE FOR EACH DAY.
3. INCLUDES: WORK INCLUDES, FLAGGERS PRESENT DURING CONSTRUCTION OPERATIONS WITH 10' OF THE RAILROAD RIGHT–OF–WAY.

SECTION 12,050 – RAILROAD INSURANCE

1.08 – MEASUREMENT AND PAYMENT
1. MEASUREMENT: MEASUREMENT WILL BE LUMP SUM.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE PER LUMP SUM.
3. INCLUDES: MEETING INSURANCE REQUIREMENTS OF CANADIAN PACIFIC DME RAILROAD.

SECTION 12,060 – REMOVABLE FLOOD GATE

1.08 – MEASUREMENT AND PAYMENT
1. MEASUREMENT: MEASUREMENT WILL BE LUMP SUM.
2. PAYMENT: PAYMENT WILL BE MADE AT THE UNIT PRICE PER LUMP SUM.
3. INCLUDES: WORK INCLUDES, BUT IS NOT LIMITED TO, PROVIDING AND INSTALLING REMOVABLE FLOOD GATE PER FLOOD CONTROL INTERNATIONAL MORESBY SLOT – IN BARRIER SYSTEM.

SECTION 12,070 – CONSTRUCTION CONTINGENCY

1.08 – MEASUREMENT AND PAYMENT
1. MEASUREMENT: CONTRACTOR SHALL INCLUDE A LUMP SUM PRICE OF \$50,000.00 IN THE BID FOR THIS ITEM.
2. PAYMENT: THIS ITEM IS INTENDED FOR USE IN CASE OF UNFORESEEN CONDITIONS RESULTING IN THE NEED FOR CONTRACT MODIFICATIONS. THE ALLOWANCE MAY BE USED TO SUPPLEMENT PAY ITEM QUANTITY INCREASES AT CONTRACT UNIT PRICES OR FORCE ACCOUNT CHANGES IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS.
ANY ALLOWANCE REMAINING UPON COMPLETION OF THE PROJECT WILL BE ADJUSTED WHEN PREPARING FINAL CHANGE ORDER FOR ADJUSTMENTS TO PLAN QUANTITIES.



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IOWA
WISCONSIN

OWNER/DEVELOPER:
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226 WEST 4TH STREET
DAVENPORT, IA 52801

PROJECT AND LOCATION:
UNION STATION FLOOD REPAIRS
DAVENPORT, IOWA

DRAWN BY: JMP
APPROVED BY: RH
DATE: 03/03/26
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
SUPPLEMENTAL SPECIFICAITONS

SET TYPE: PRELIMINARY
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JOB NUMBER:
19–717PH05

SHEET NUMBER:
C.02



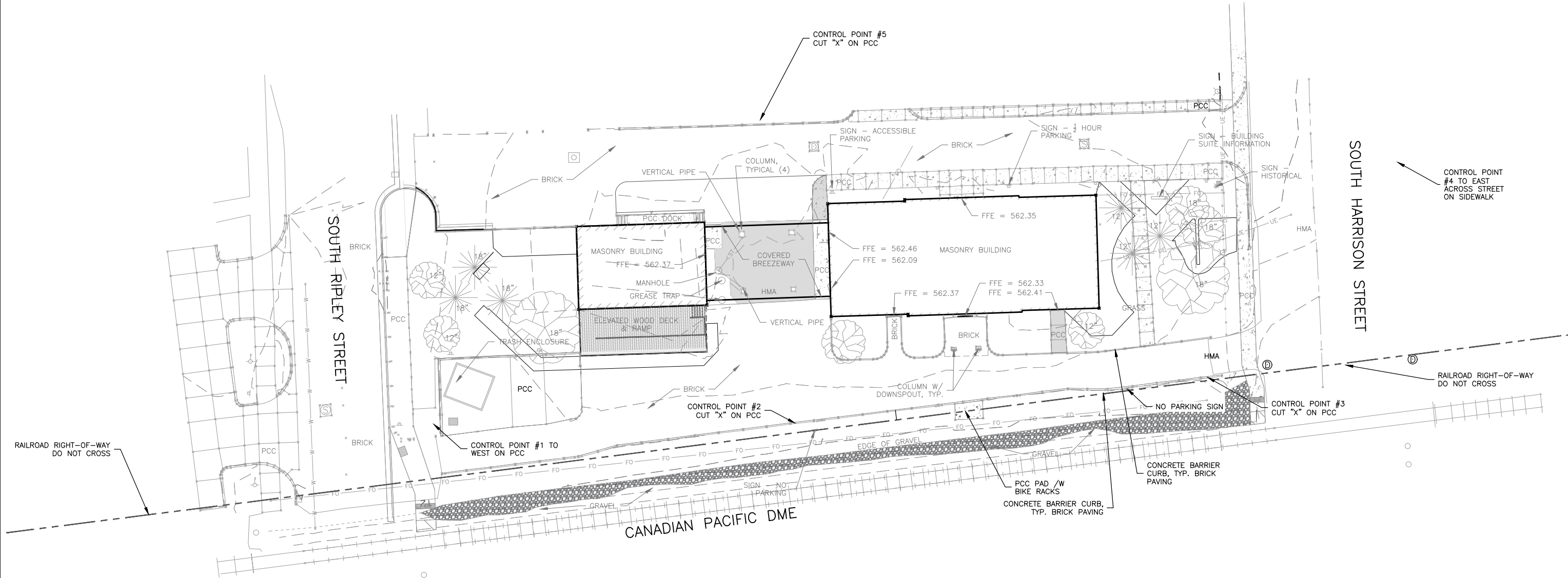
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WEST RIVER DRIVE

SOUTH HARRISON STREET

SOUTH RIPLEY STREET

CANADIAN PACIFIC DME



CONTROL POINTS

CONTROL POINT #1, CUT "X" ON PCC	NORTHING: 567091.84	EASTING: 2440336.85	ELEVATION: 560.62
CONTROL POINT #2, CUT "X" ON PCC	NORTHING: 567092.27	EASTING: 2440483.63	ELEVATION: 560.95
CONTROL POINT #3, CUT "X" ON PCC	NORTHING: 567110.88	EASTING: 2440651.11	ELEVATION: 561.05
CONTROL POINT #4, CUT "X" ON SIDEWALK	NORTHING: 567198.28	EASTING: 2440728.20	ELEVATION: 560.11
CONTROL POINT #5, CUT "X" ON SIDEWALK	NORTHING: 567215.45	EASTING: 2440469.45	ELEVATION: 562.33

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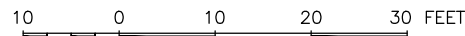
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EXISTING CONDITIONS

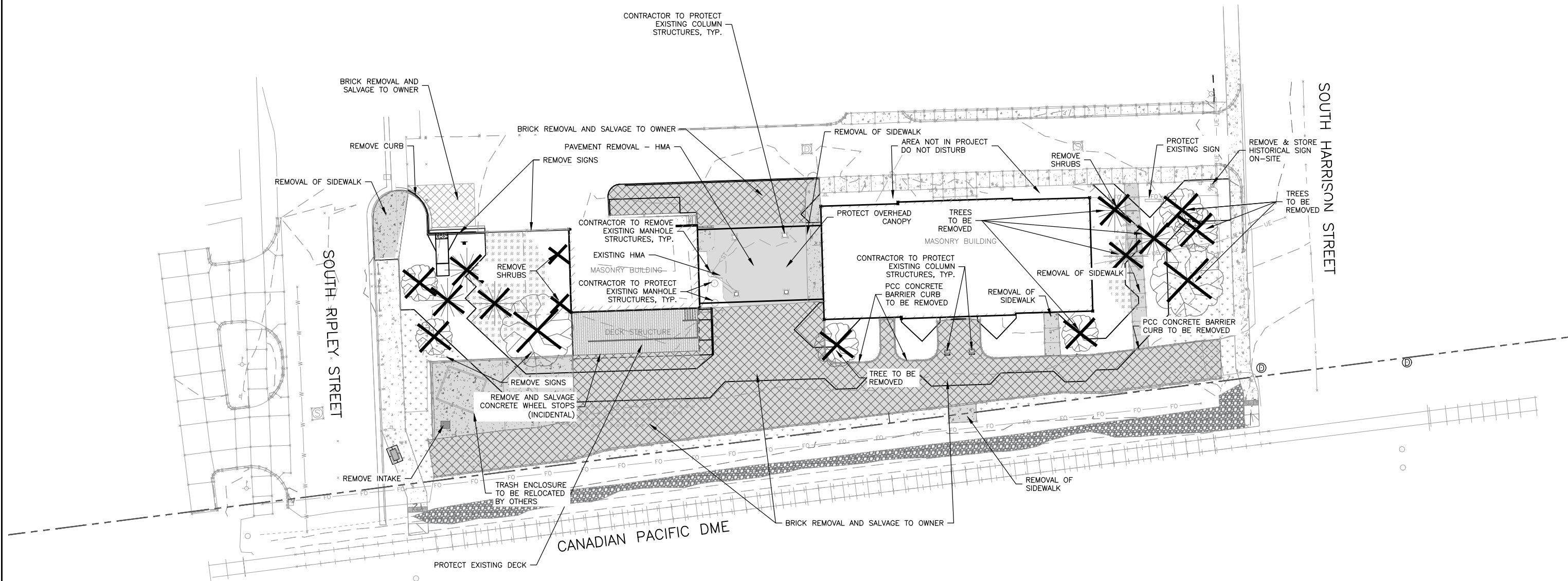
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WEST RIVER DRIVE



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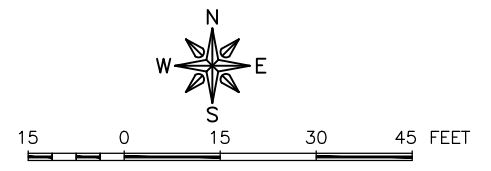
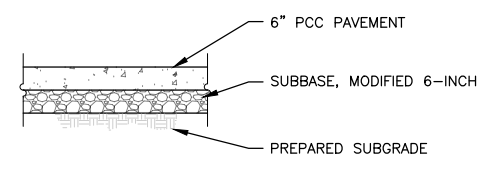
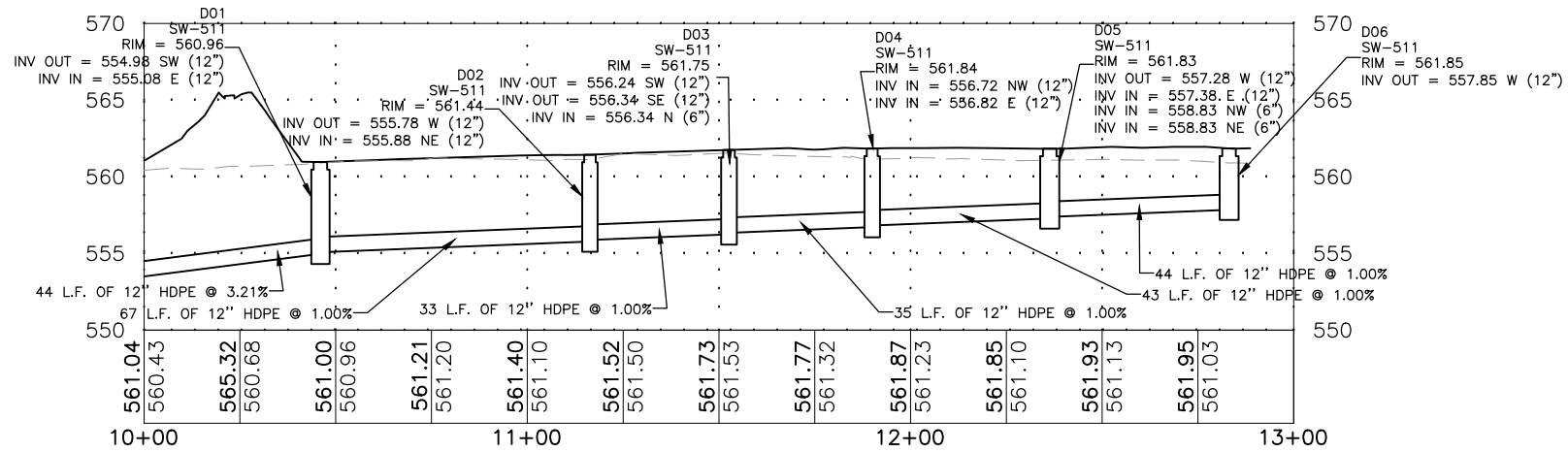
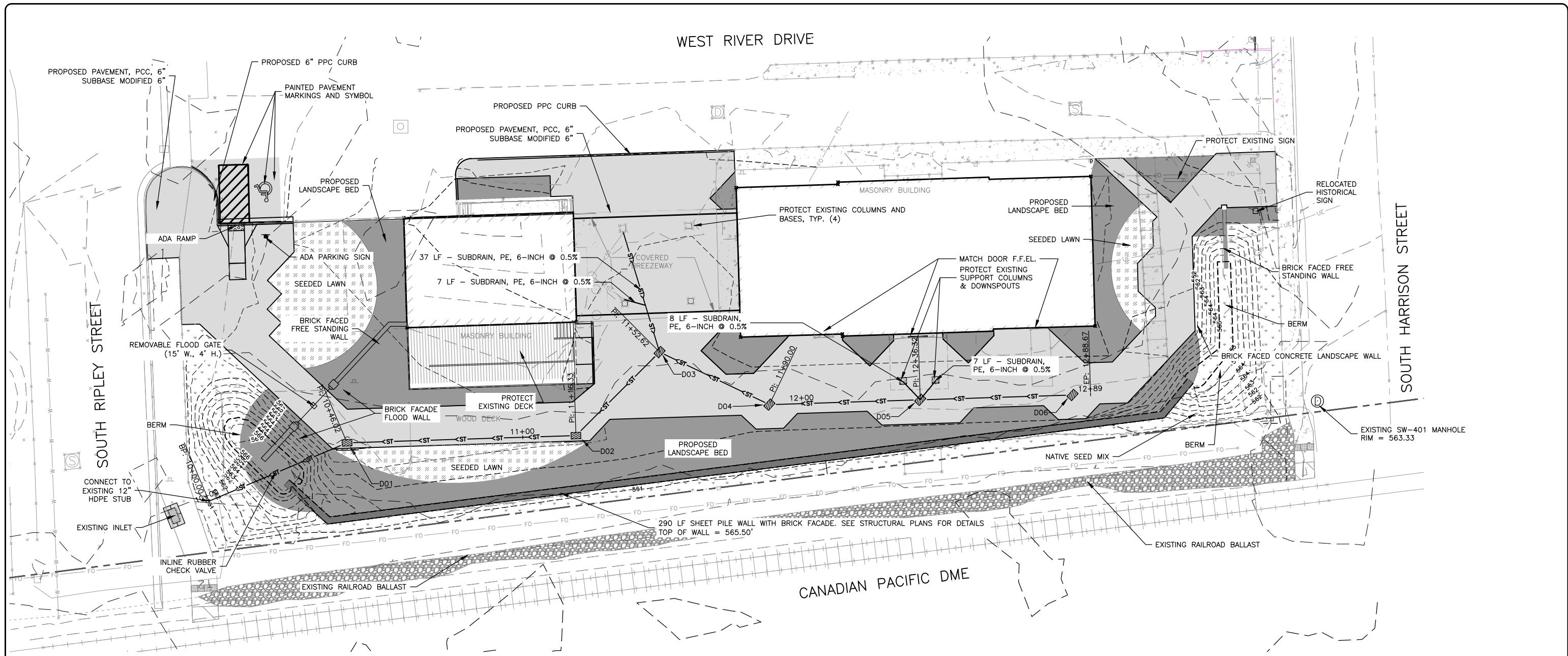
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REMOVAL PLAN

SET TYPE: PRELIMINARY

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SHEET NUMBER:
C.04



CONCRETE PAVEMENT CROSS SECTION
N.T.S.

LEGEND
6" PCC

NOTES:
INLINE RUBBER CHECK VALVE SHALL BE TIDEFLEX R CHECKMATE R BY RED VALVE OR APPROVED EQUAL

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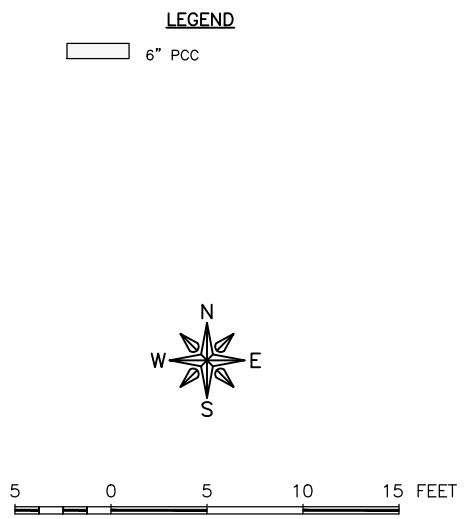
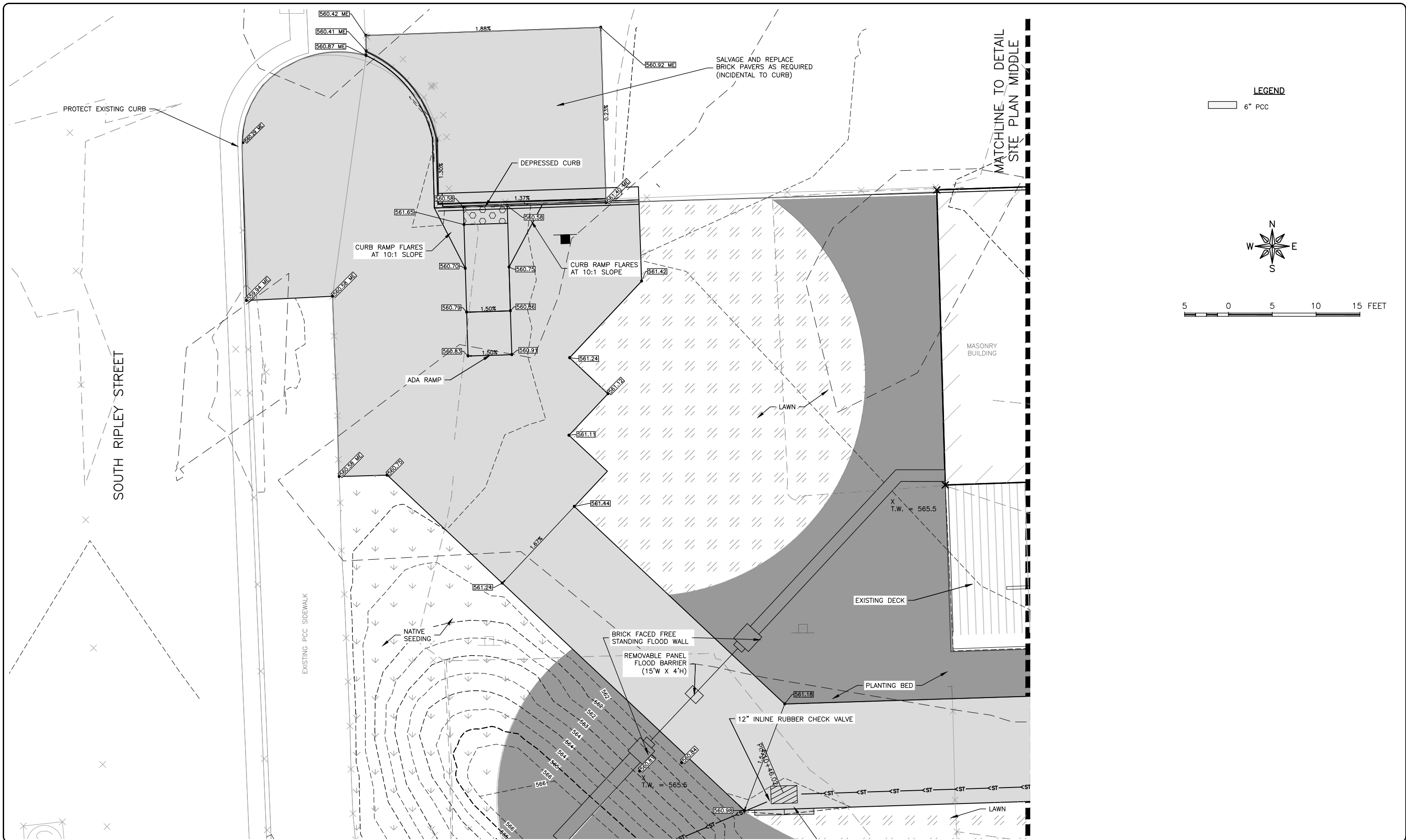
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DAVENPORT, IOWA

DRAWN BY: JMP
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SET TYPE: PRELIMINARY
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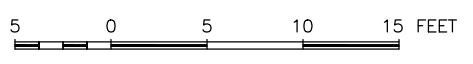
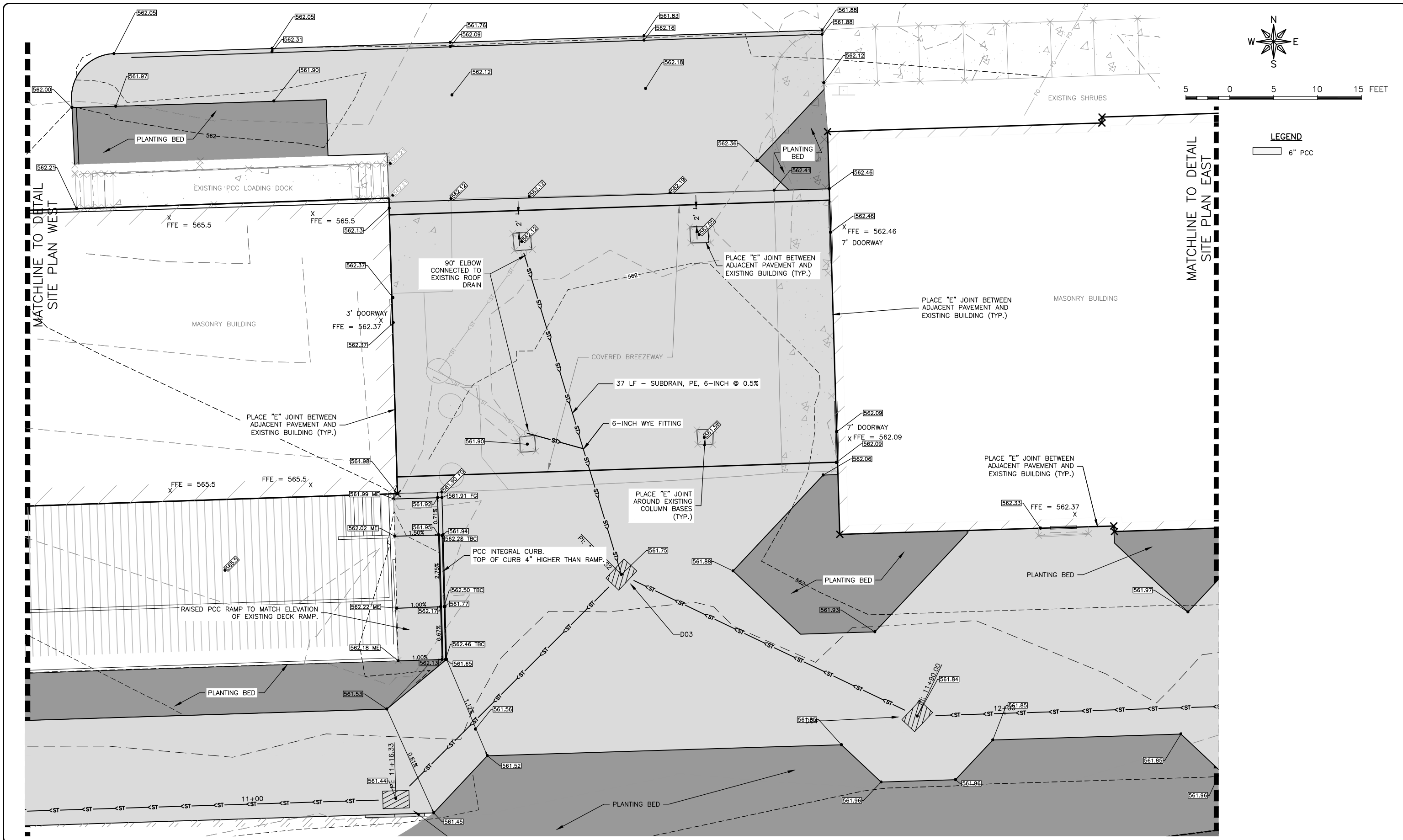
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DETAIL SITE PLAN WEST

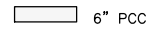
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19-717PH05

SHEET NUMBER:
D.02



LEGEND



MATCHLINE TO DETAIL SITE PLAN WEST

MATCHLINE TO DETAIL SITE PLAN EAST

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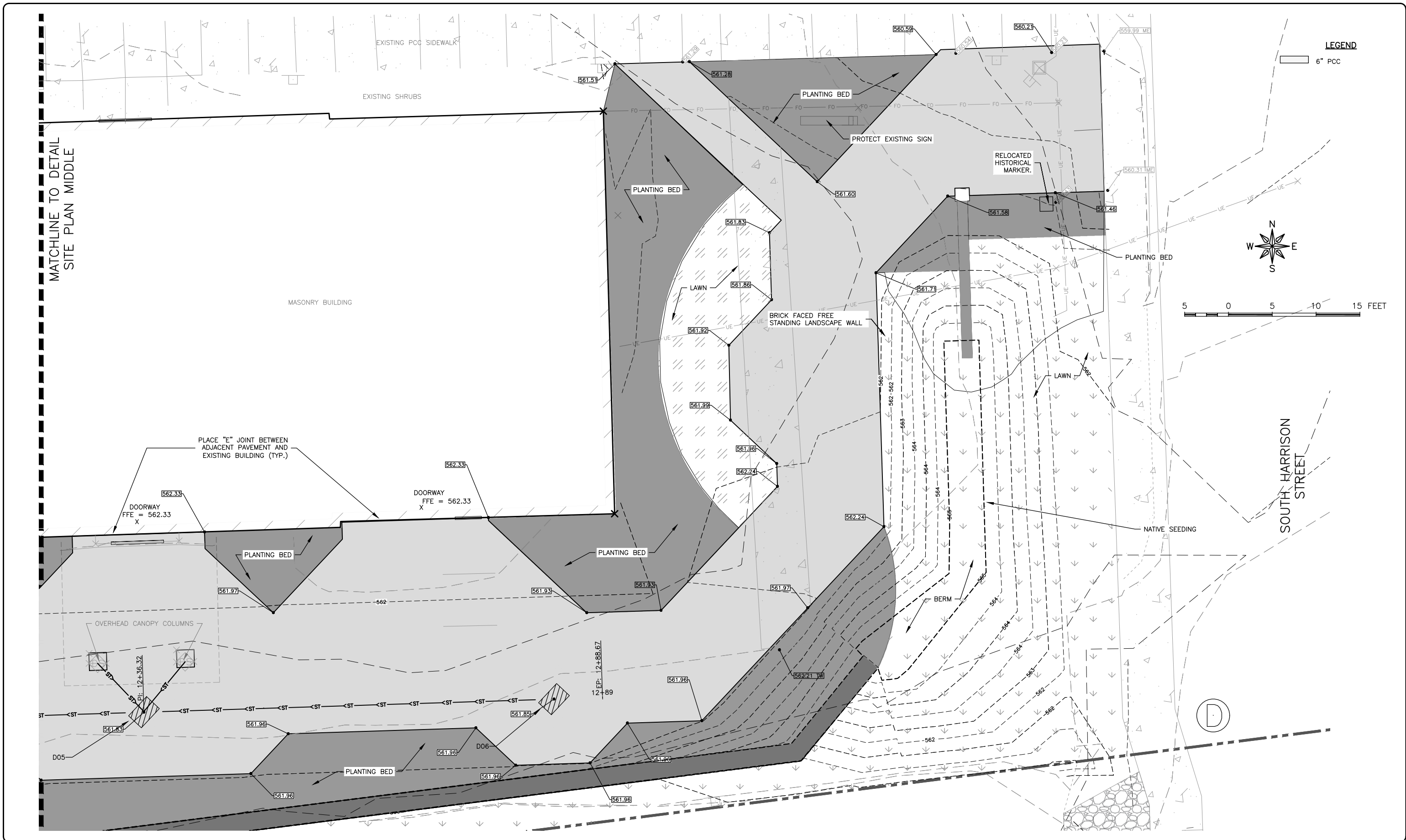
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DRAWING:
DETAIL SITE PLAN MIDDLE

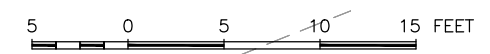
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JOB NUMBER:
19-717PH05

SHEET NUMBER:
D.03



LEGEND
 6" PCC



MATCHLINE TO DETAIL
 SITE PLAN MIDDLE

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REVISIONS		
REV. NO.	DESCRIPTION	DATE

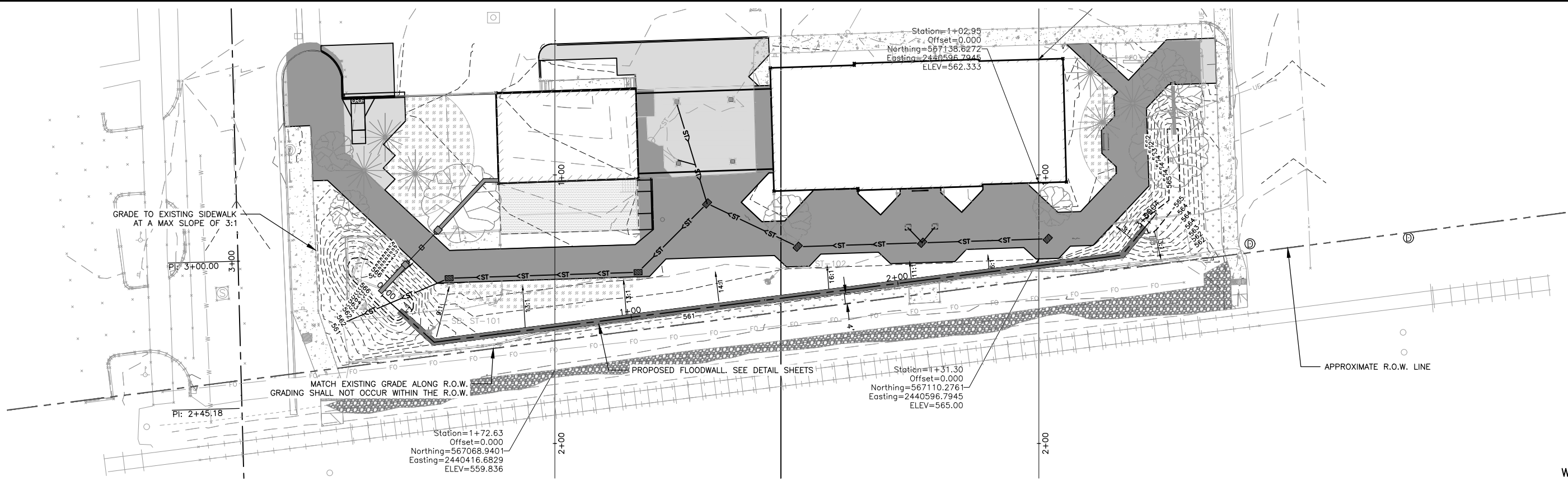
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 DETAIL SITE PLAN EAST

SET TYPE: PRELIMINARY

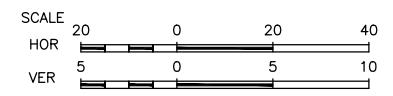
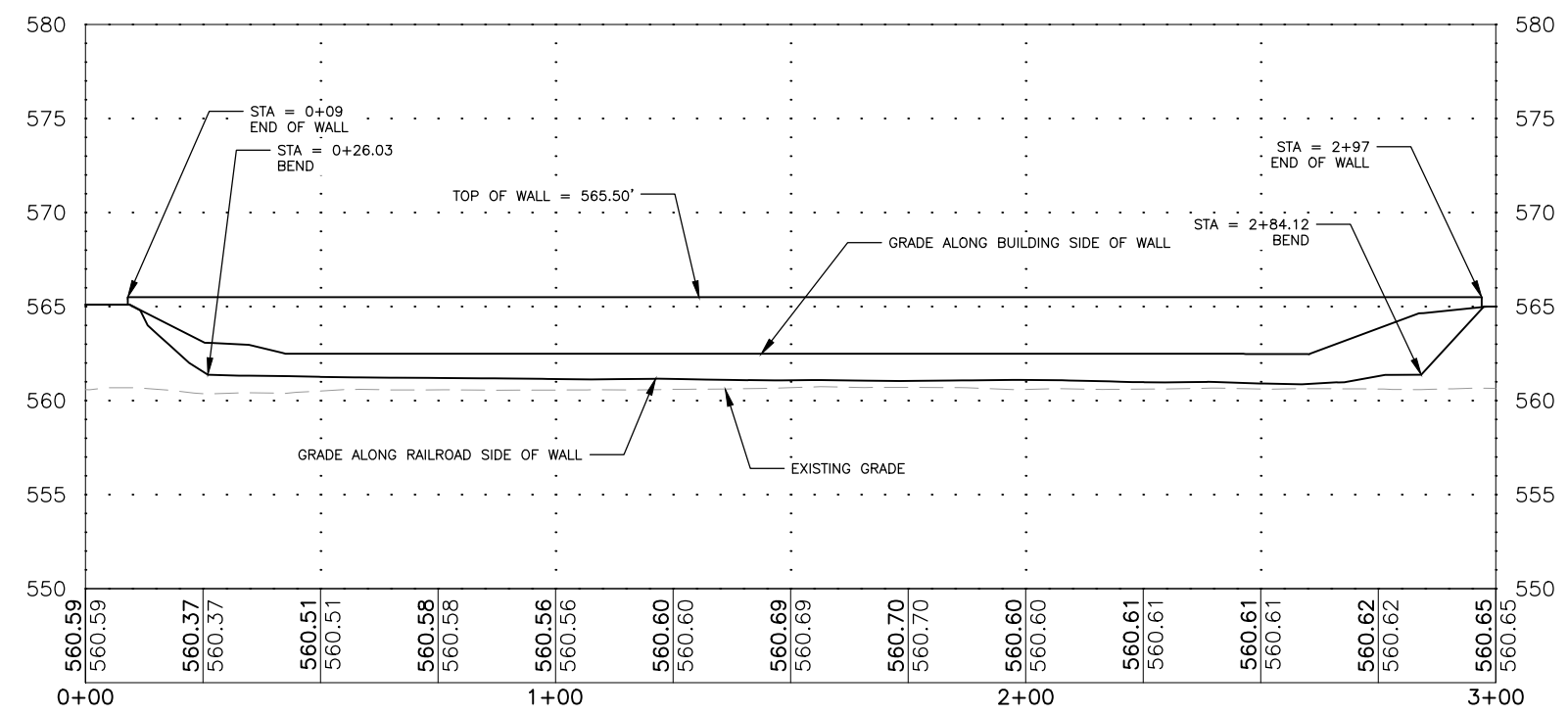
\\rockford\Drawings\C30\19-717 Davenport Flood Repairs\Plans\19-717 PH05 Plans.dwg, D.04

JOB NUMBER:
 19-717PH05

SHEET NUMBER:
 D.04



PROPOSED FLOOD WALL PROFILE



FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL

ILLINOIS
IOWA
WISCONSIN

OWNER/DEVELOPER:
CITY OF DAVENPORT
226 WEST 4TH STREET
DAVENPORT, IA 52801

PROJECT AND LOCATION:
UNION STATION FLOOD REPAIRS
DAVENPORT, IOWA

DRAWN BY: JMP
APPROVED BY: RH
DATE: 03/03/26
SCALE: AS NOTED

REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
DETAIL FLOOD WALL PLAN

SET TYPE: PRELIMINARY

JOB NUMBER:
19-717PH05

SHEET NUMBER:
D.05

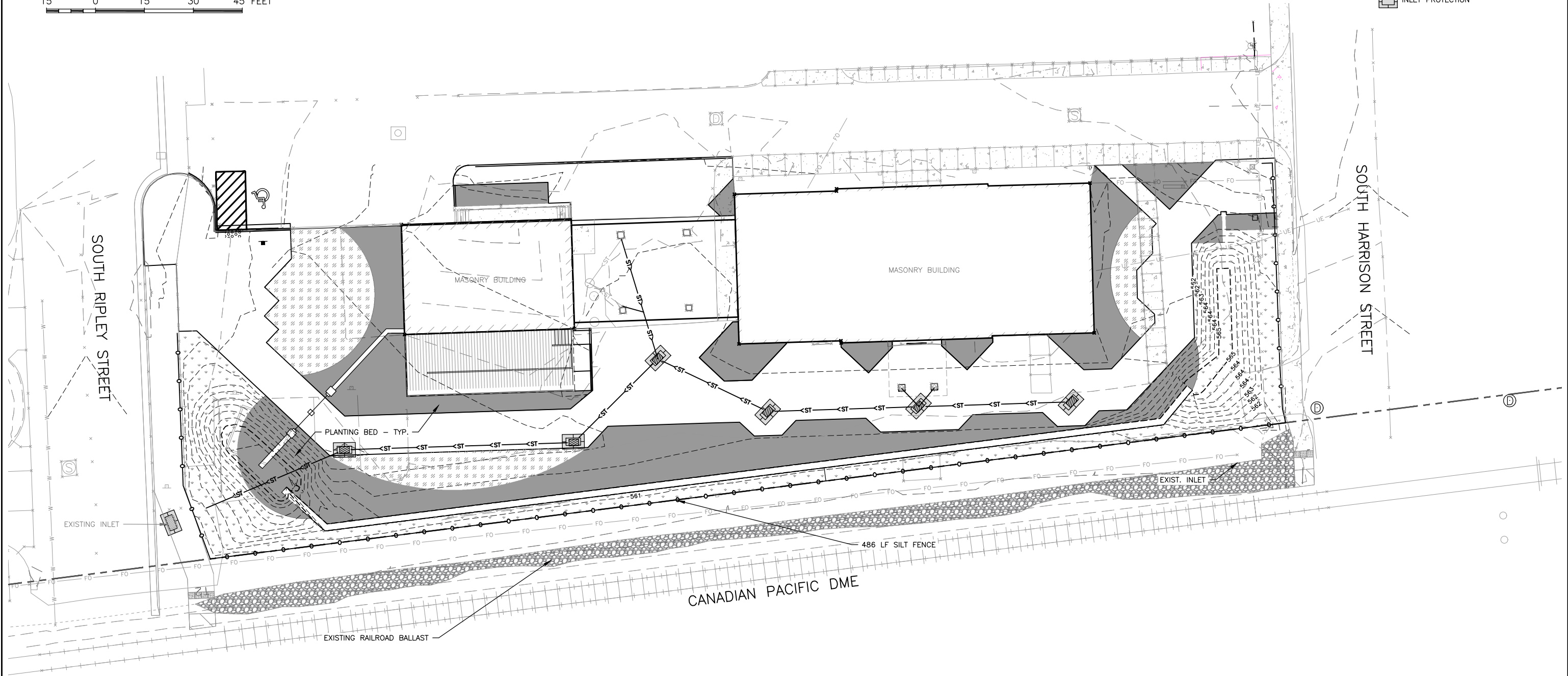


WEST RIVER DRIVE

15 0 15 30 45 FEET

LEGEND

- SEED W/EROSION CONTROL FABRIC
- SEED W/EROSION CONTROL FABRIC
- PLANTING BEDS W/MULCH
- PERIMETER SILT FENCE
- INLET PROTECTION



FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL

ILLINOIS
IOWA
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OWNER/DEVELOPER:
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226 WEST 4TH STREET
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PROJECT AND LOCATION:
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DAVENPORT, IOWA

DRAWN BY: JMP
APPROVED BY: RH
DATE: 03/03/26
SCALE: AS NOTED

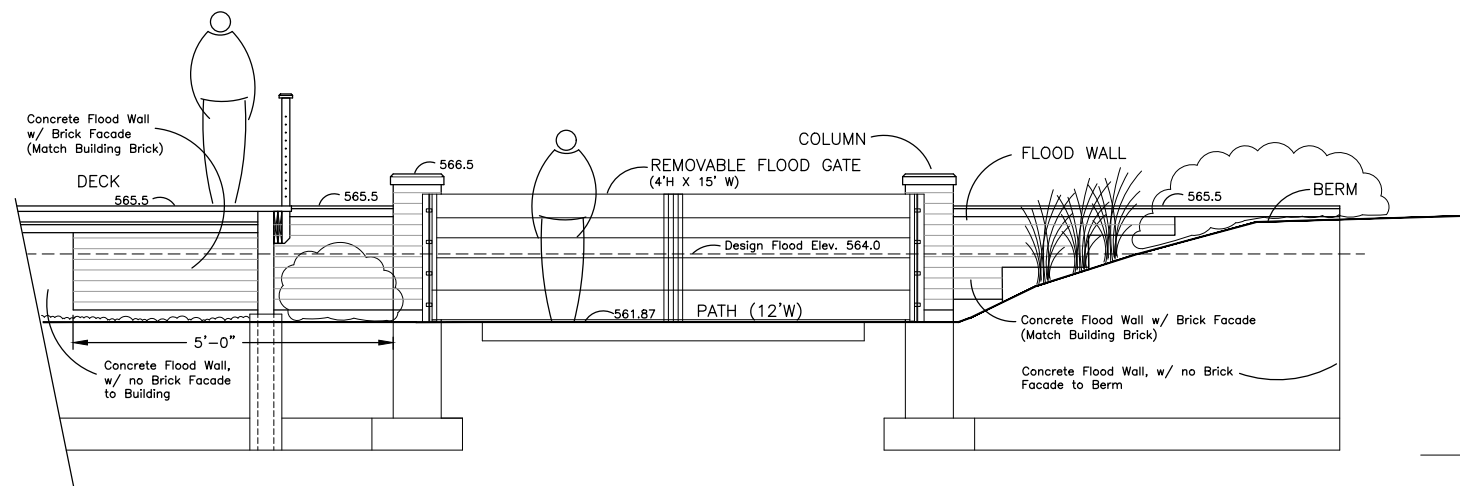
REVISIONS		
REV. NO.	DESCRIPTION	DATE

DRAWING:
SEDIMENT AND EROSION CONTROL PLAN

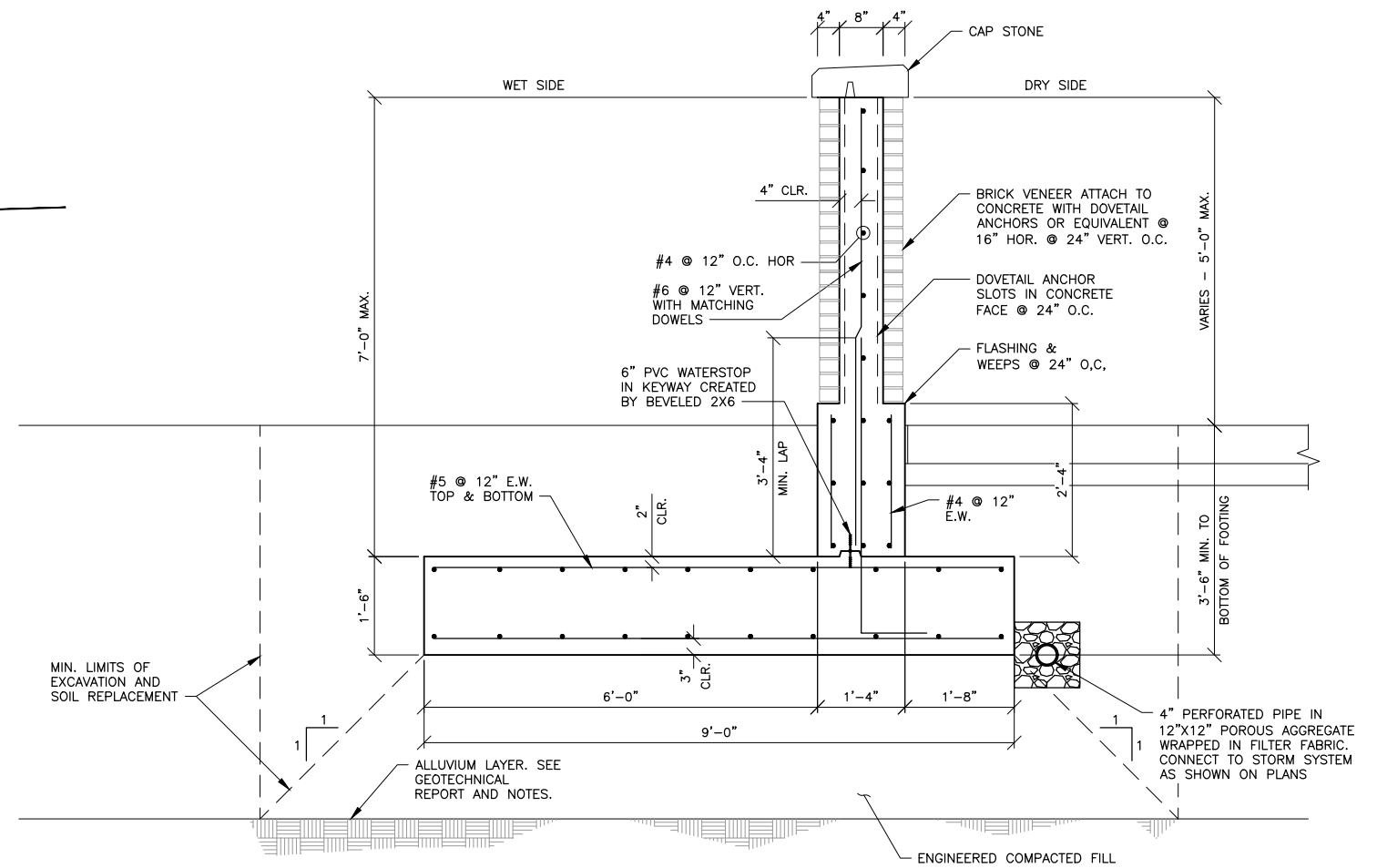
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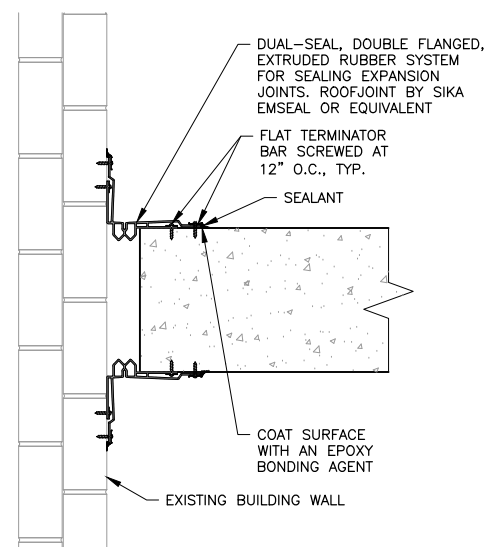
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R.01



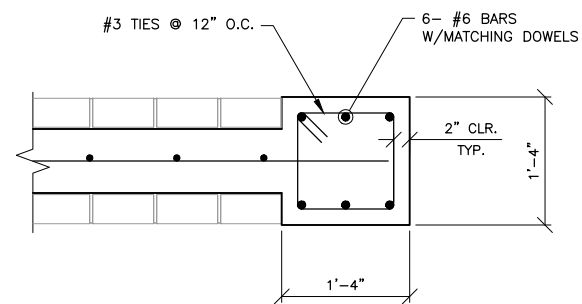
FLOODGATE AND FLOOD WALL ELEVATION
LOOKING SOUTH EAST
N.T.S.



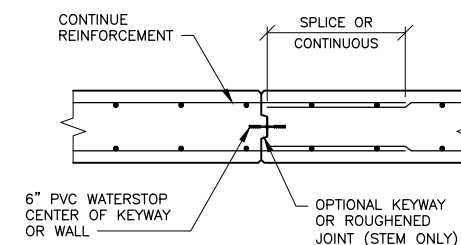
BRICK FACED FREESTANDING FLOODWALL
N.T.S.



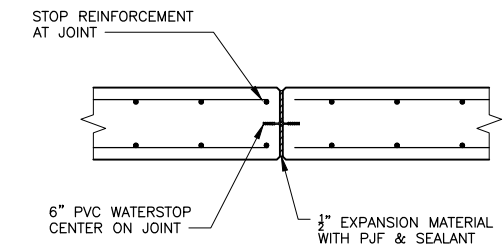
FLOODWALL TO EXISTING BUILDING
CLOSURE DETAIL
N.T.S.



PILASTER AT REMOVABLE BARRIER
N.T.S.

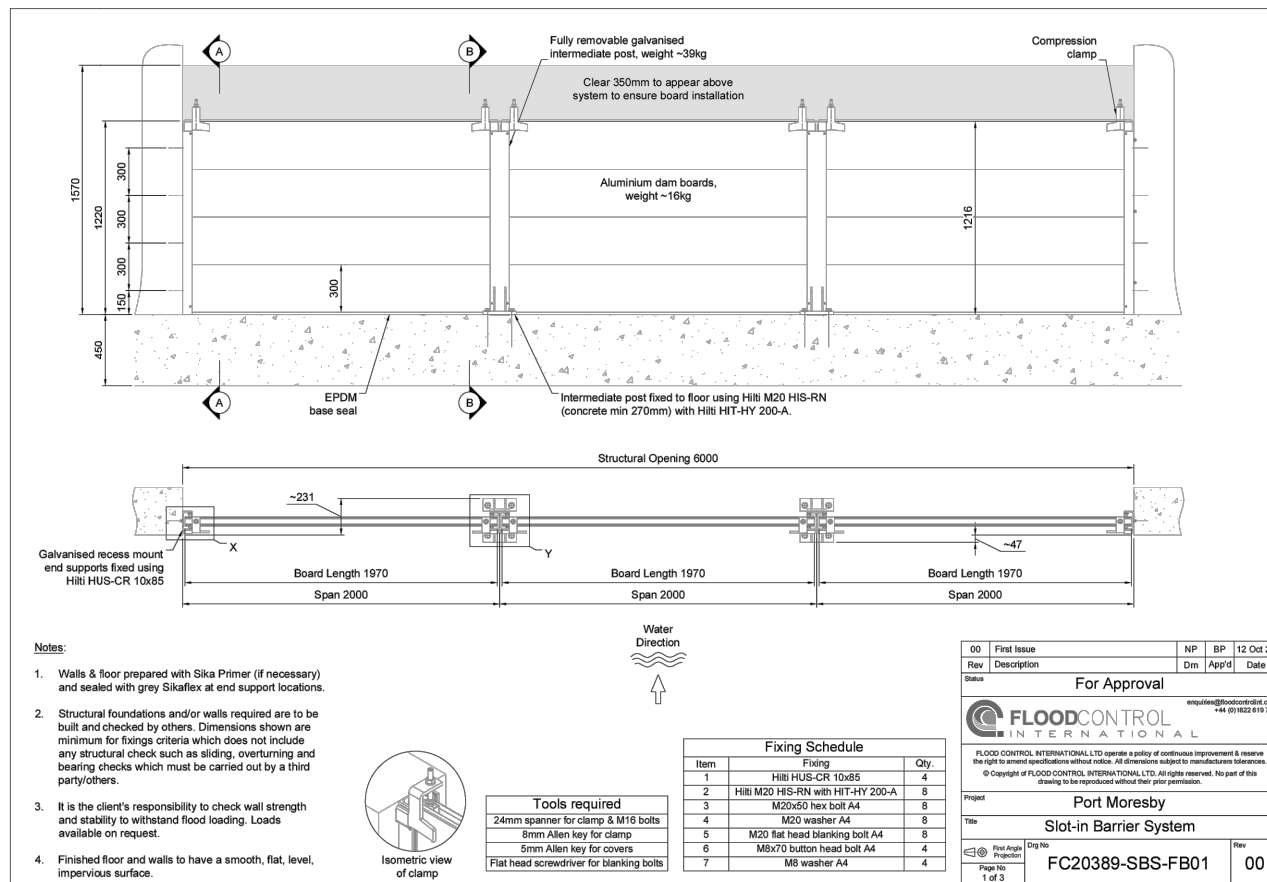


CONCRETE WALL CONTRACTION JOINT
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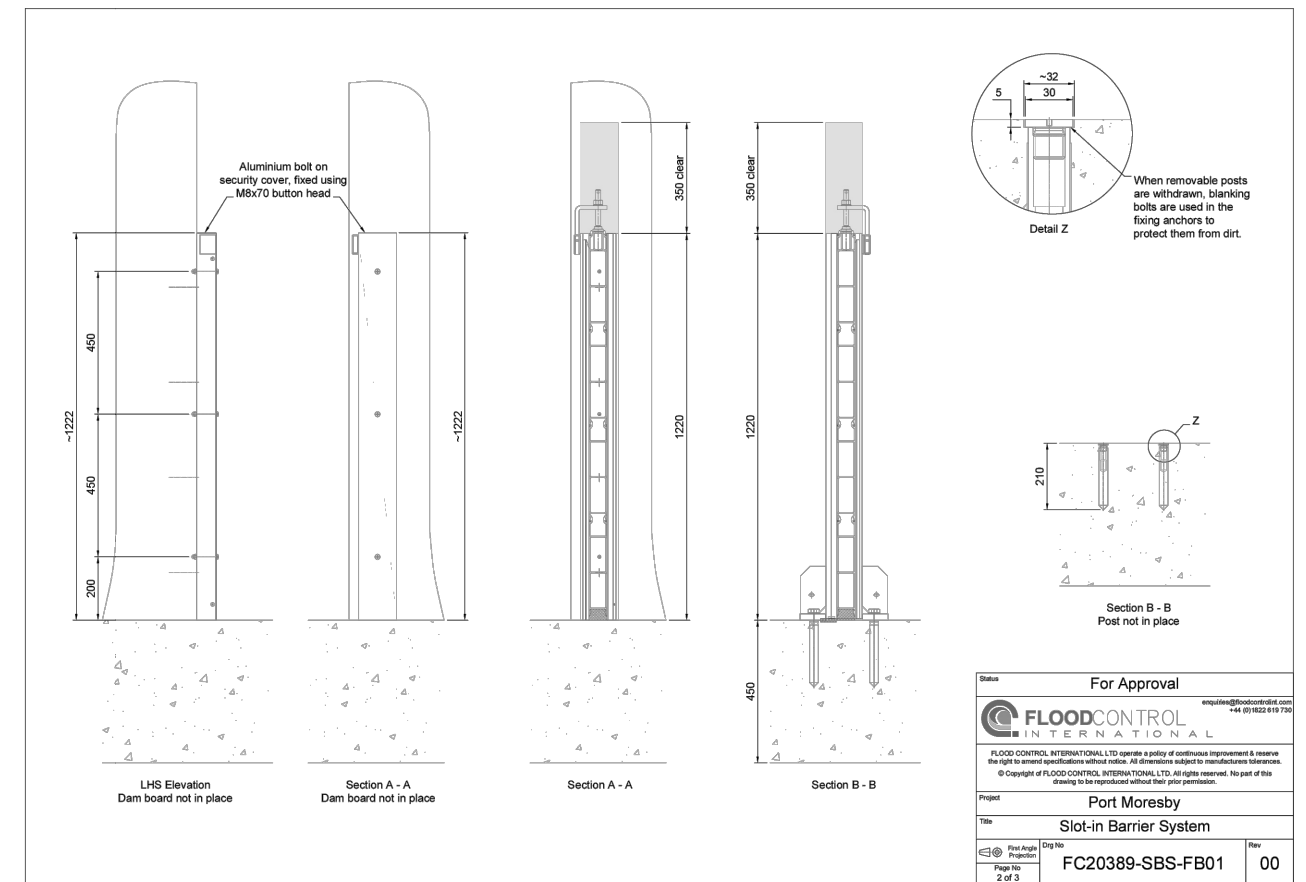


CONCRETE WALL EXPANSION JOINT
N.T.S.

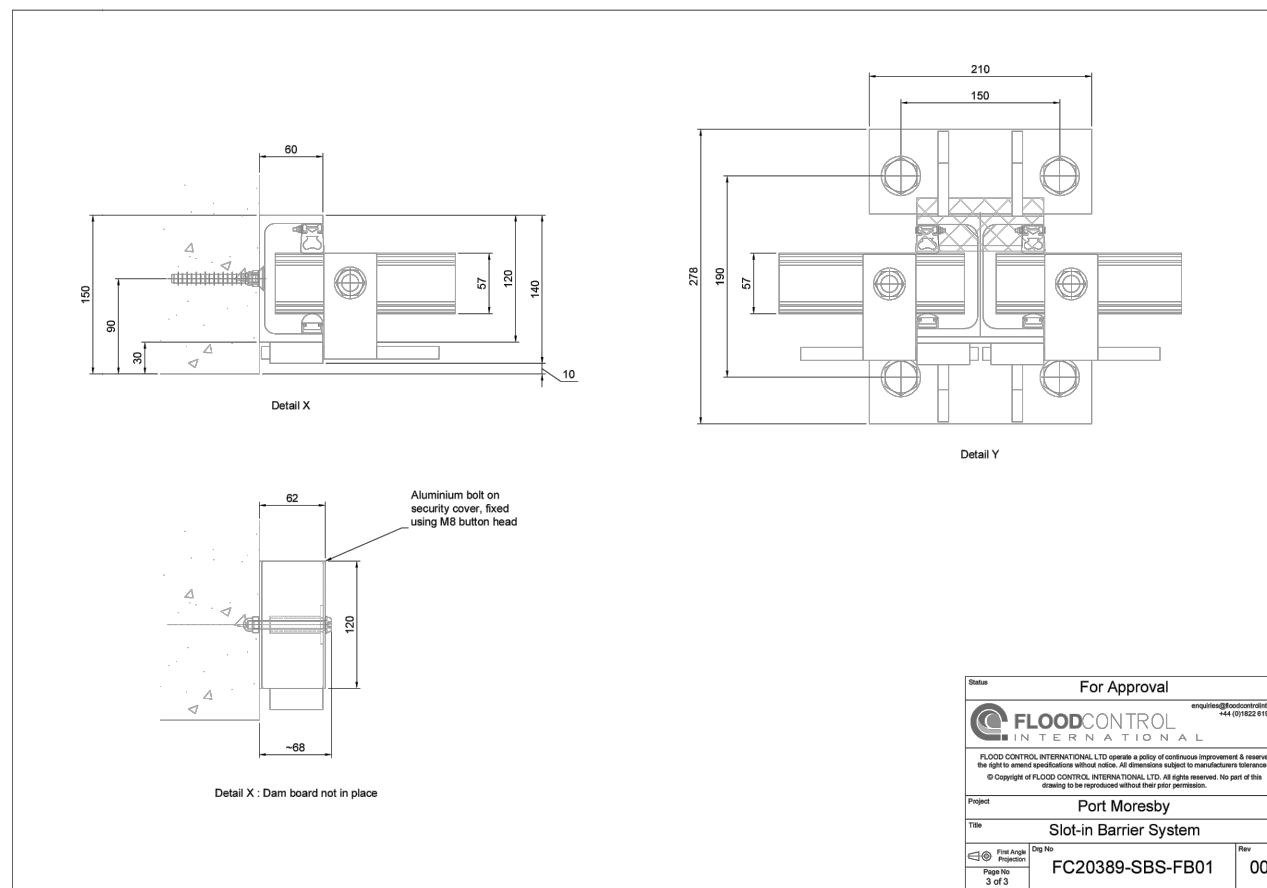
REVISIONS		
REV. NO.	DESCRIPTION	DATE



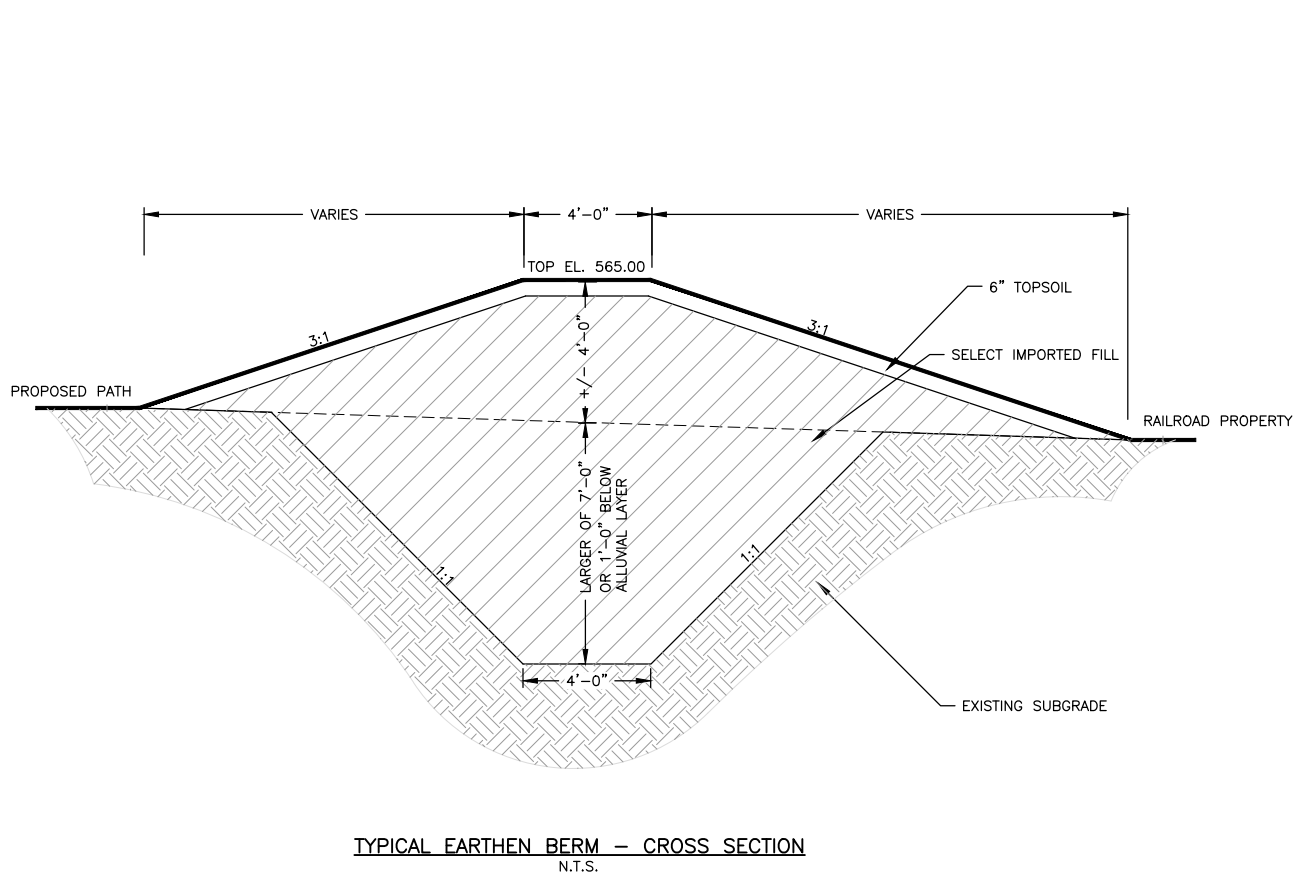
NOTE: DETAIL FOR REFERENCE ONLY. SEE PLANS FOR PROJECT-SPECIFIC INSTALLATION ON REQUIREMENTS.



NOTE: DETAIL FOR REFERENCE ONLY. SEE PLANS FOR PROJECT-SPECIFIC INSTALLATION ON REQUIREMENTS.



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REVISIONS		
REV. NO.	DESCRIPTION	DATE

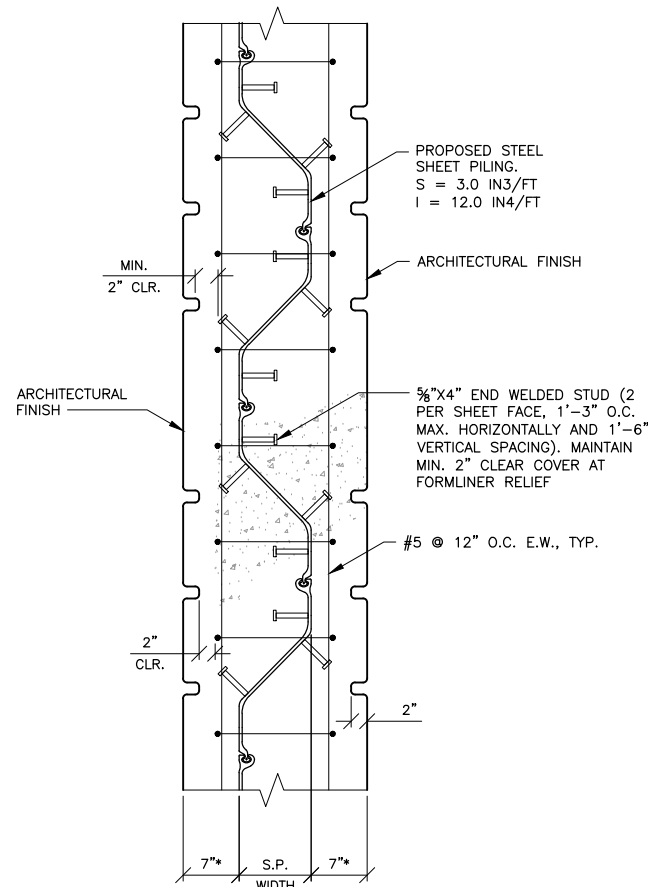
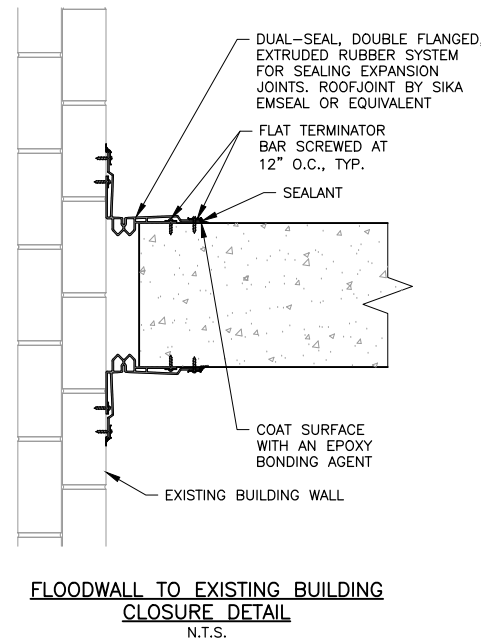
LOG OF BORING

Project Number B2008714
Geotechnical Evaluation
Union Station Flood Protection
102 South Harrison Street
Davenport, Iowa

BORING: ST-103
LOCATION: See attached sketch
NORTHING: 2440632 EASTING: 567113
START DATE: 10/21/20 END DATE: 10/21/20
SURFACING: Grass WEATHER:

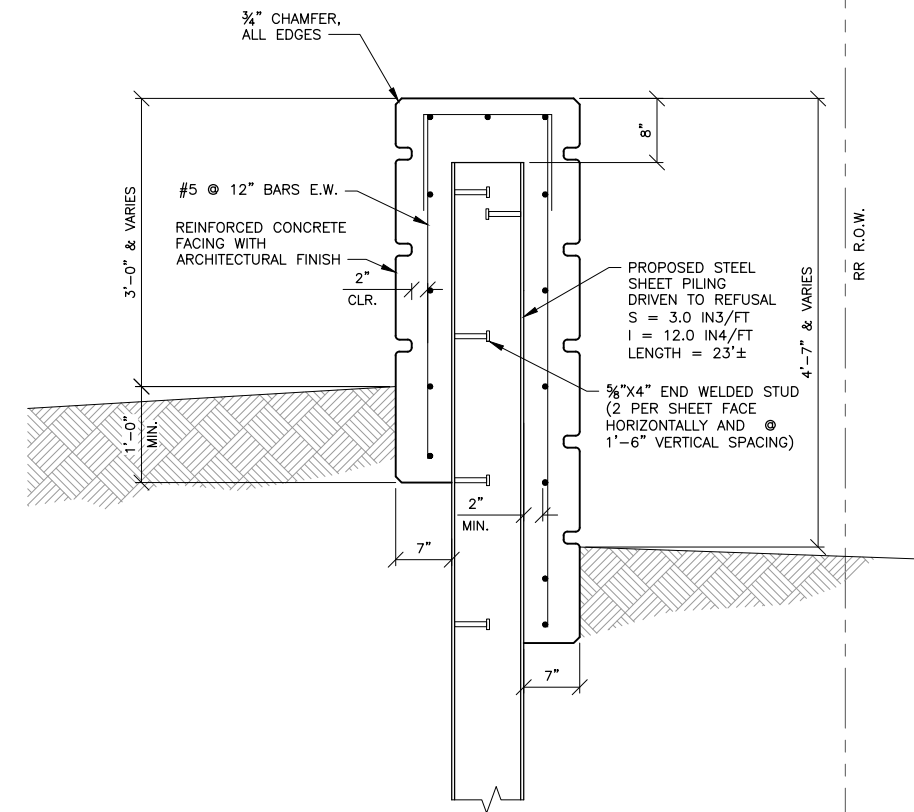
Surface Elevation	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
559.5	1.0	TOPSOIL FILL: SANDY LEAN CLAY (CL), trace debris, and organics, dark brown to gray, moist	SH 24" 2-2-1 (3) 5"	4.4	19	LL=39, PL=30, PI=9 P200=46% q _u =0.5 tsf
553.5	7.0	SILT (ML), gray, moist (ALLUVIUM)	1-1-1 (2) 14"	6.1	14	Nonplastic P200=30%
549.5	11.0	LEAN CLAY (CL), trace Gravel, gray, moist, stiff (ALLUVIUM)	SH 24" 3-5-7 (12) 5"	3.5	18	P200=49%
546.5	14.0	SHALE, gray, highly weathered, hard, (BEDROCK)	10-15-27 (42) 9"	6.5	14.9	
542.5	18.0	END OF BORING Boring immediately backfilled	19-50/4" (REF) 10"			Auger met refusal at 18 feet Water not observed while drilling.

Braun Intertec Corporation Print Date: 11/27/2020 ST-103 page 1 of 1



*NOTE:
WIDTH IS BASED ON 2" FORMLINER RELIEF.
ADJUST WIDTH AS NECESSARY.

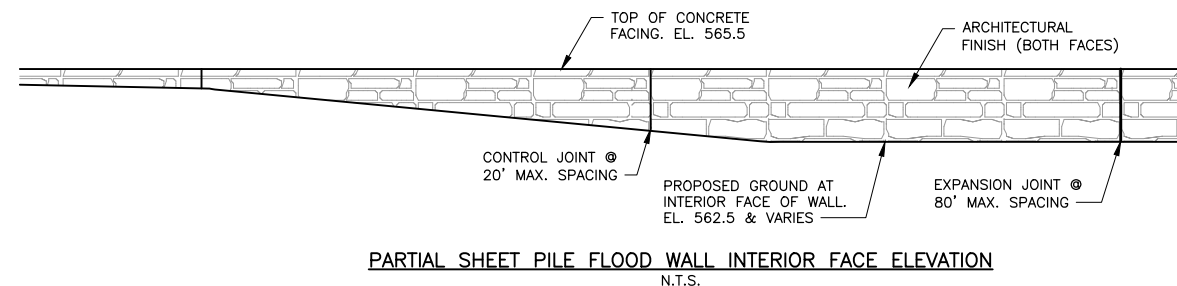
PARTIAL PLAN OF SHEET PILE
N.T.S.



TYPICAL SHEET PILE FLOOD WALL SECTION
N.T.S.

SHEET PILE WALL NOTES:

- DESIGN BASED ON SOIL BORINGS AND GEOTECHNICAL REPORT B2008714 BY BRAUN INTERTEC. COPY IS AVAILABLE FROM THE ENGINEER.
- STEEL SHEET PILES AND ANY PREFABRICATED SHAPES SHALL BE OF A DESIGN THAT ENSURES CONTINUOUS INTERLOCK THROUGHOUT THE ENTIRE LENGTH OF THE WALL WHEN IN PLACE.
- STEEL SHEET PILING SHALL MEET THE REQUIREMENTS OF ASTM A328 OR ASTM A572 GRADE 50.
- SHEAR STUDS SHALL CONFORM TO ASTM A29/A108 (GRADES 1010-1020) WITH A MINIMUM TENSILE STRENGTH OF 60 KSI.
- ALL WELDING SHALL BE ACCORDING TO AWS D1.1 WITH E70XX ELECTRODES.
- HANDLING HOLES SHALL BE PLUGGED BY WELDING A PIECE OF STEEL OVER THE HOLE PRIOR TO POURING THE CONCRETE FACING AND SHALL BE WATERTIGHT.
- ALL FABRICATED CONNECTIONS SHALL BE MADE WITH THE USE OF ANGLES OR BENT PLATES AND SHALL BE ADEQUATELY WELDED FOR A WATERTIGHT CONNECTION.
- PILES SHALL BE DRIVEN TO REFUSAL, DEFINED AS 10 BLOWS PER INCH WITH AN APPROVED PILE HAMMER.
- ESTIMATED PILE LENGTH IS 23 FEET.
- REQUIRED MIN. ELASTIC SECTION MODULUS = 3.0 IN3/FT
- REQUIRED MIN. MOMENT OF INERTIA = 12.0 IN4/FT
- HOT ROLLED Z SECTIONS ARE REQUIRED FOR REDUCED PERMEABILITY.
- STEEL SHEET PILING SHALL BE DRIVEN TO FORM A TIGHT BULKHEAD.
- SHEET PILES SHALL BE DRIVEN TO FORM A STRAIGHT LINE BETWEEN DEFLECTION POINTS WITH THE MAXIMUM DEVIATION FROM A STRAIGHT LINE BETWEEN THESE POINTS BEING 4".
- EACH SHEET PILE SECTION SHALL BE PLUMB TO WITHIN 2% OF ANY VERTICAL LENGTH.
- TOP OF SHEET PILE SECTIONS SHALL BE WITHIN 2" FROM ELEVATIONS SHOWN ON THE PLANS.
- STEEL SHEET PILING SHOP DRAWINGS ARE TO BE SUBMITTED.
- CONCRETE FACING TO HAVE A MIN. COMPRESSIVE STRENGTH OF 3,500 PSI AND REINFORCEMENT BARS TO BE GRADE 60 KSI.
- INSTALL CONTROL AND EXPANSION JOINTS IN CONCRETE FACING SIMILAR TO CONCRETE WALL DETAILS.
- ARCHITECTURAL FINISH OF CONCRETE FACING SHALL BE OF A PATTERN, TEXTURE AND COLOR AS SELECTED BY THE OWNER. PATTERN AND TEXTURE SHALL BE PRODUCED BY A COMMERCIAL FORM LINER. COLOR SHALL BE ACHIEVED BY AN INTEGRAL COLOR PIGMENT AND BY A CONCRETE PENETRATING STAIN MATCHING THE COLOR OF THE INTEGRAL PIGMENT.
- A SEPARATE 3'X3' COMPLETELY FINISHED SAMPLE OF THE CONCRETE FACING PATTERN AND COLOR TO BE SUBMITTED FOR OWNERS APPROVAL.



LOG OF BORING

Project Number B2008714
Geotechnical Evaluation
Union Station Flood Protection
102 South Harrison Street
Davenport, Iowa

BORING: ST-101
LOCATION: See attached sketch
NORTHING: 2440380 EASTING: 567002
START DATE: 10/21/20 END DATE: 10/21/20
SURFACING: Pavement WEATHER:

Surface Elevation	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
559.8	0.7	CONCRETE FILL: SANDY LEAN CLAY (CL), trace Gravel, and debris, dark brown, moist	9-2-6 (8) 0"	12.1	28	LL=96, PL=19, PI=17 P200=67% DD=95 pcf e=0.79 Uc=1000 pcf
557.5	3.0	LEAN CLAY (CL), trace Sand, brown and gray, moist to wet, soft to stiff (ALLUVIUM) Hydrocarbon-like odor	SH 24" 3-2-1 (3) 10"	9.1	26	LL=32, PL=21, PI=11 P200=11%
547.0	13.5	POORLY GRADED GRAVEL with SILT (GP-GM), fine to coarse-grained, gray and brown, moist, medium dense (ALLUVIUM)	SH 0" 9-9-11 (20) 9"	29.0	8	P200=12%
544.5	16.0	SHALE, gray, highly weathered, hard, (BEDROCK)	19-24-32 (56) 17"	17.9		Auger met refusal at 18 feet Water observed at 13.5 feet while drilling. Hydrocarbon-like odor from water on auger
542.5	18.0	END OF BORING Boring immediately backfilled				

Braun Intertec Corporation Print Date: 11/27/2020 ST-101 page 1 of 1

REVISIONS		
REV. NO.	DESCRIPTION	DATE

LANDSCAPE NOTES:

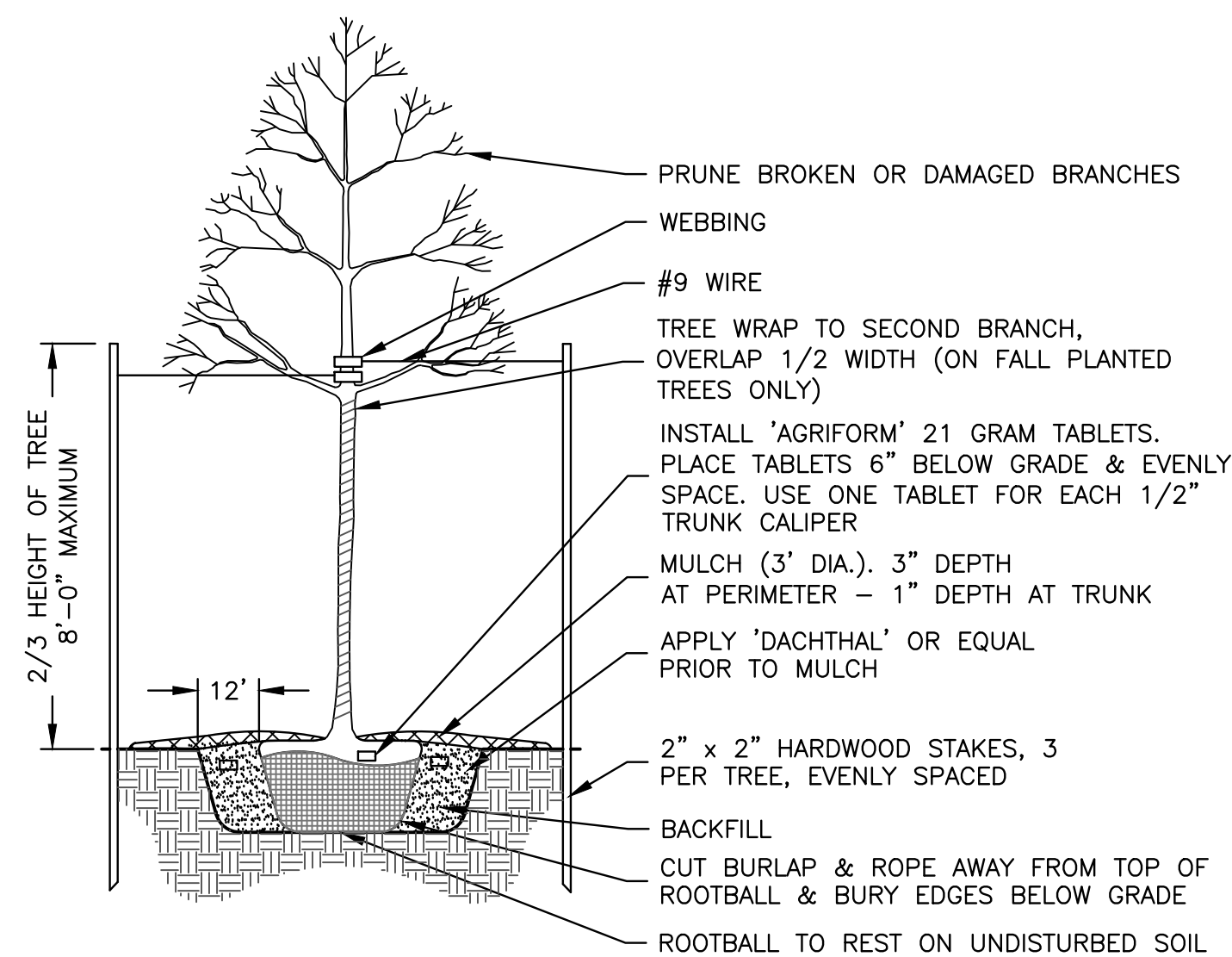
- CONTRACTOR SHALL VISIT SITE, INSPECT EXISTING CONDITIONS AND REVIEW PROPOSED WORK. IN CASE OF DISCREPANCY BETWEEN PLAN AND QUANTITY LISTS, PLAN SHALL GOVERN QUANTITIES.
- CONTRACTOR SHALL VERIFY LOCATIONS OF ALL ON-SITE UTILITIES PRIOR TO BEGINNING CONSTRUCTION ON HIS/HER PHASE OF WORK. ELECTRIC, GAS, TELEPHONE, AND CABLE TELEVISION MAY BE LOCATED BY CALLING 811 NATIONWIDE TO REQUEST A LINE LOCATE. ANY DAMAGE TO UTILITIES OR INTERRUPTION OF SERVICES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR TO COORDINATE ALL RELATED ACTIVITIES WITH OTHER TRADES ON THE JOB AND SHALL REPORT ANY UNACCEPTABLE JOB CONDITIONS TO OWNER'S REPRESENTATIVE PRIOR TO COMMENCING WORK.
- CONTRACTOR RESPONSIBLE FOR APPLICATION AND COST OF ALL NECESSARY PERMITS AND CODE VERIFICATIONS. SUBMIT COPIES OF ALL DOCUMENTS TO OWNER AND LANDSCAPE ARCHITECT.
- ALL SHRUB BEDS AND TREES SHALL BE MULCHED WITH A 3" DEPTH CONTINUOUS LAYER OF DARK BROWN SHREDDED HARDWOOD BARK MULCH. ALL GROUND COVER AND PERENNIAL BEDS SHALL BE MULCHED WITH A 1" DEPTH LAYER OF MULCH. ALL DECIDUOUS TREES SHALL BE MULCHED WITH A 3' DIAMETER CIRCLE OF 3" DEPTH MULCH. ALL EVERGREEN TREES SHALL BE MULCHED TO THE DRIP LINE.
- PERENNIAL AND GROUND COVER BEDS SHALL BE AMENDED WITH A 2" LAYER OF MUSHROOM COMPOST, TILLED TO A DEPTH OF 6", RAKED SMOOTH, FERTILIZED WITH COMMERCIAL 10-6-4 FERTILIZER AT A RATE OF 25 LBS. PER 1,000 S.F., PLANTED, COVERED WITH 1" LAYER OF DARK BROWN SHREDDED HARDWOOD BARK MULCH AND WATERED. CONTRACTOR SHALL APPLY A PRE-EMERGENT HERBICIDE IN ALL PLANTING BEDS.
- EDGE ALL SHRUB BEDS THAT ABUTS LAWN WITH EDG-KING BRAND PLASTIC EDGING OR APPROVED EQUAL. EDGING TO BE INSTALLED WITH HORIZONTAL STEEL STAKES AT 36" SPACING. INSTALL PER MANUFACTURER'S SPECIFICATIONS IN ALL AREAS INDICATED ON PLAN. PROVIDE MANUFACTURED JOINTS AND 90° FITTINGS ALL CORNERS.
- THE TOPSOIL CONDITION FOR THIS PROJECT SITE IS AS FOLLOWS:
CONTRACTOR TO SUPPLY, PLACE AND FINISH GRADE TOPSOIL AT SPECIFIED DEPTHS IN PLANTING AND LAWN AREAS. PLANTING AREAS: 12 INCHES, LAWN AREAS: 6 INCHES.
- LANDSCAPE ARCHITECT DURING AND AT THE END OF THE GUARANTEE PERIOD.
- SEEDED LAWN TO BE A LOW MOW FESCUE MIX. ACCEPTANCE AND GUARANTEE NOTES SHALL APPLY TO ALL SEEDED AREAS.
- ACCEPTANCE OF GRADING AND SEEDING SHALL BE BY LANDSCAPE ARCHITECT AND OWNER. THE

CONTRACTOR SHALL ASSUME MAINTENANCE RESPONSIBILITIES FOR A MINIMUM OF SIXTY (60) DAYS OR UNTIL SECOND CUTTING, WHICHEVER IS LONGER. MAINTENANCE SHALL INCLUDE WATERING, WEEDING, RESEEDING AND OTHER OPERATIONS NECESSARY TO KEEP LAWN IN A THRIVING CONDITION. UPON FINAL ACCEPTANCE BY LANDSCAPE ARCHITECT AND/OR OWNER, OWNER SHALL ASSUME ALL MAINTENANCE RESPONSIBILITIES. AFTER LAWN AREAS HAVE GERMINATED, AREAS WHICH FAIL TO SHOW A UNIFORM STAND OF GRASS FOR ANY REASON WHATSOEVER SHALL BE RESEEDED REPEATEDLY UNTIL ALL AREAS ARE COVERED WITH A SATISFACTORY STAND OF GRASS. MINIMUM ACCEPTANCE OF SEEDED LAWN AREAS MAY INCLUDE SCATTERED BARE SPOTS, NONE OF WHICH ARE LARGER THAN 1 SQUARE FOOT AND WHEN COMBINED DO NOT EXCEED 2% OF TOTAL SEEDED LAWN AREA.

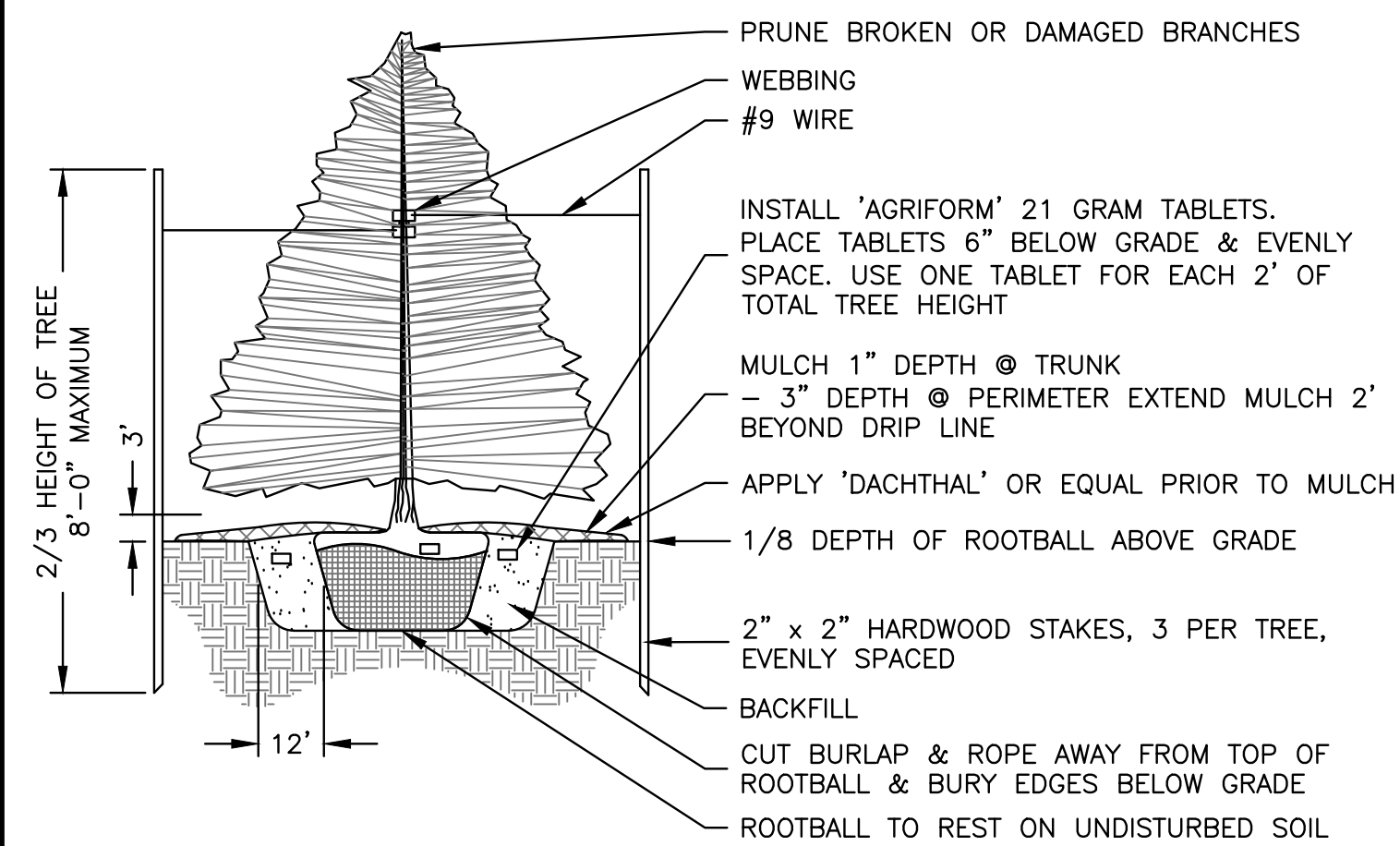
- CONTRACTOR TO SUPPLY ALL LABOR AND MATERIALS NECESSARY TO INSTALL NATIVE SEED MIX IN AREAS INDICATED ON PLAN. MIX TO BE A SHORT GRASS PRAIRIE (MESIC SOIL) BY AGREGOL WWW.AGRECOL.COM OR APPROVED EQUAL. INSTALL AS PER MANUFACTURER'S RECOMMENDATIONS.
- CONTRACTOR TO SUPPLY ALL LABOR AND MATERIALS NECESSARY TO INSTALL EROSION CONTROL BLAKET IN ALL SEEDED AREAS. BLANKET TO BE S-75 ERONET BY NORTH AMERICAN GREEN OR APPROVED EQUAL. INSTALL AS PER MANUFACTURER'S RECOMMENDATIONS.
- CONTRACTOR TO PROVIDE NATIVE SEED POST PLANTING MAINTENANCE & ESTABLISHMENT
 YEAR ONE
 1. WHEN WEEDS REACH 8"-10". FIRST MOW TO BE AT A HEIGHT OF 4"-5". REMAINDER OF FIRST SEASON, MOW HEIGHT TO BE 6"-8" (APPROX. 3-4 TIMES DURING FIRST GROWING SEASON)
 2. STOP MOWING AT THE END OF FIRST SEASON.
 3. DO NOT SPRAY ANY BROADLEAF CHEMICALS IN THE FIRST SEASON.
 YEAR TWO
 1. IF WEEDS ARE THICK IN THE BEGINNING OF THE SECOND SEASON, MOW OR SPOT-MOW ONCE OR TWICE. RAISE THE MOW HEIGHT TO 6"-12".
 2. OVERSEED BARE PATCHES AS NEEDED. (APRIL-JUNE) KEEP OVERSEEDED AREAS MOIST FOR FIRST 3 WEEKS.
 3. DO NOT SPRAY ANY BROADLEAF CHEMICALS IN THE FIRST SEASON.
 YEAR THREE
 1. MOW IN EARLY SPRING.
 2. SPOT TREATMENT WITH BROADLEAF CHEMICALS MAY BE APPLIED.

LANDSCAPE SCHEDULE:

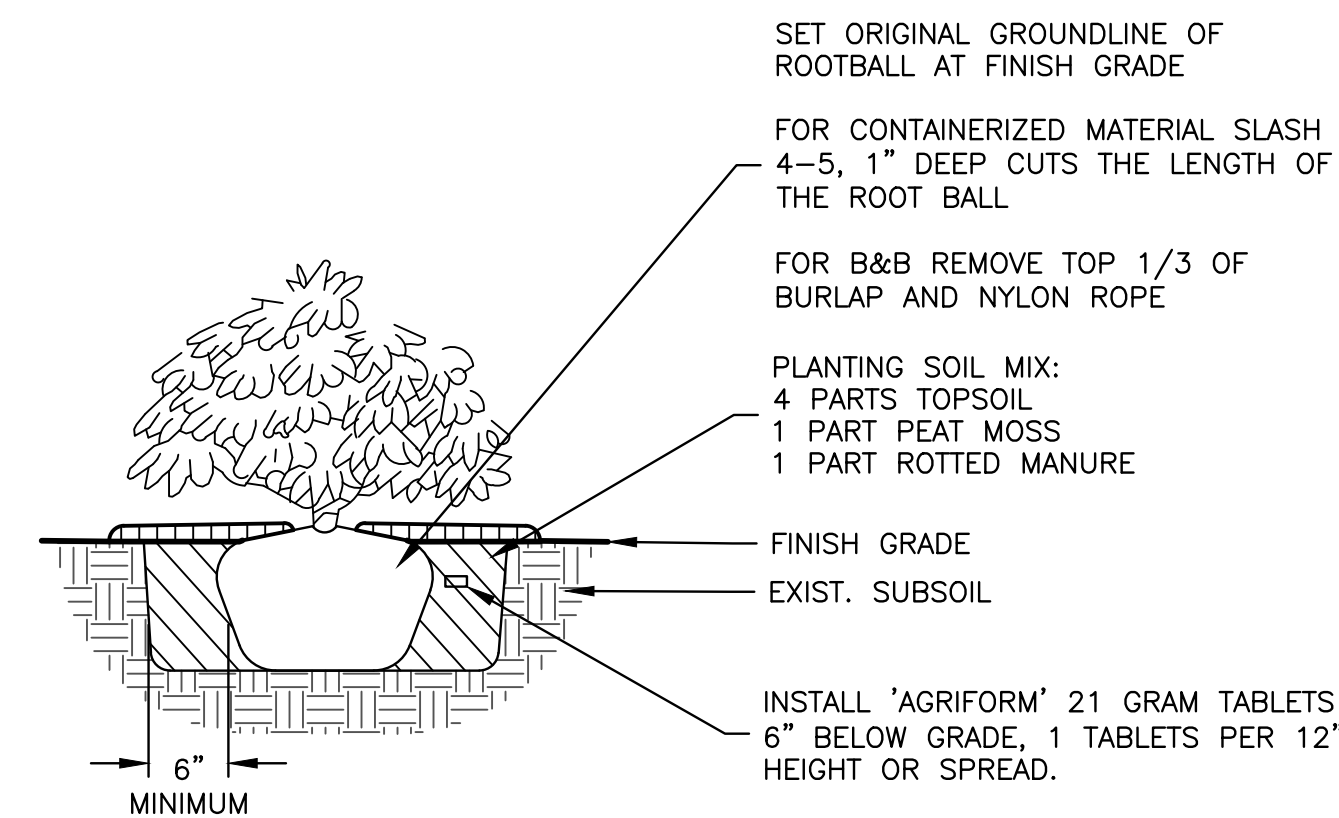
KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS	TYPE
		TREES				
AG	3	Acer griseum	PAPERBARK MAPLE	8' HT.	SINGLE-STEM	SHADE TREE
SJT	4	Syringa reticulata	JAPANESE TREE LILAC	5' HT.	MULTI-STEM	Ornamental Tree
AB	1	Amelanchier x grandiflora 'Autumn Brilliance'	AUTUMN BRILLIANCE SERVICEBERRY	5' HT.	MULTI-STEM	Ornamental Tree
MLN	6	Magnolia liliiflora 'Nigra'	ANNE MAGNOLIA	5' HT.	MULTI-STEM	Ornamental Tree
CBC	3	Carpinus betulus 'Columnaris'	COLUMNAR EUROPEAN HORNBEAM	8' HT.	SINGLE-STEM	Ornamental Tree
		SHRUBS				
JSM	31	Juniperus sabna 'Monna'	Calgary Carpet Juniper	5 GAL.		Evergreen Shrub
CSS	31	Clethra alnifolia 'Summer Spark'	SUMMER SPARK SUMMERSWEET	24"		Deciduous Shrub
DRK	19	Diervilla rivularis 'Kodiak'	KODIAK RED BUSH HONEYSUCKLE	24"		Deciduous Shrub
SNF	36	Spiraea japonica 'Neon Flash'	NEON FLASH SPIREA	24"		Deciduous Shrub
		PERENNIALS, GROUNDCOVER & ORNAMENTAL GRASSES				
CGB	144	Carex flacca	BLUE SEDGE	1 GAL.	2' O.C.	Perennial, Groundcover, Ornamental Grass
CKF	36	Calamagrostis x acutiflora 'Karl Foerster'	KARL FOERSTER FEATHER REED GRASS	3 GAL.	3' O.C.	Perennial, Groundcover, Ornamental Grass
HMO	48	Heuchera micrantha 'Obsidian'	OBSIDIAN CORAL BELLS	1 GAL.	2' O.C.	Perennial, Groundcover, Ornamental Grass
HMS	63	Hemerocallis 'Stella-de-oro'	STELLA-DE-ORO DAYLILY	1 GAL.	2' O.C.	Perennial, Groundcover, Ornamental Grass
HS-13	101	Hosta 'Blue Angel'	BLUE ANGEL HOSTA	1 GAL.	2' O.C.	Perennial, Groundcover, Ornamental Grass
MCR	62	Monarda didyma 'Coral Reef'	CORAL REEF BEEBALM	1 GAL.	2' O.C.	Perennial, Groundcover, Ornamental Grass
PAD	52	Pennisetum alopecuroides 'Hameln'	DWARF FOUNTAIN GRASS	3 GAL.	3' O.C.	Perennial, Groundcover, Ornamental Grass



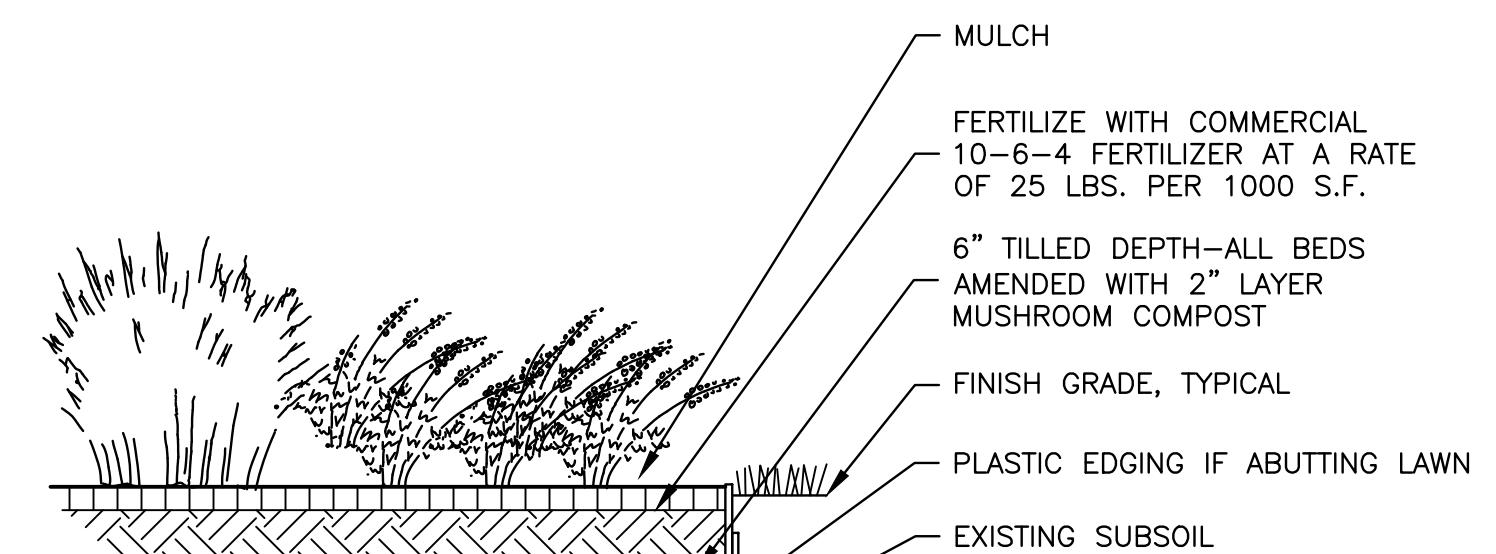
TREE PLANTING
N.T.S.



EVERGREEN TREE PLANTING
N.T.S.



SHRUB PLANTING
N.T.S.



PERENNIAL/GROUNDCOVER PLANTING
N.T.S.

REVISIONS		
REV. NO.	DESCRIPTION	DATE

City of Davenport

Department: Development & Neighborhood Services
Contact Info: Matt Werderitch | 563-888-2221

Action / Date
3/23/2026

Subject:
Election of Chairperson

Recommendation:
Nominate and elect a Chairperson.

Background:

The Bylaws and Rules of Procedure for the Design Review Board state, "The Chairperson of the Board shall be elected at the Annual Meeting of the Board and shall hold office for one year or until his/her successor is elected."

The election of a Chairperson is to take place at the January Board meeting each year.

The following responsibilities apply to the role of Chairperson:

1. The Chairperson shall preside at all meetings of the Board.
2. The Chairperson shall have general charge of the business of the Board.
3. The Chairperson shall appoint all committees.
4. The Chairperson shall perform such other duties as are usually exercised by the Chairperson of a Board or as specifically authorized by the Board.

Attachments:

1. Design Review Board Bylaws

**BYLAWS AND RULES OF PROCEDURE
DESIGN REVIEW BOARD
CITY OF DAVENPORT, IOWA**

Adopted 2-22-2021

Section 1.0 Officers.

1.1 Offices.

1.1.a Chairperson

- 1) The position of Chairperson is hereby established.
- 2) The Chairperson shall preside at all meetings of the Board.
- 3) The Chairperson shall have general charge of the business of the Board.
- 4) The Chairperson shall appoint all committees.
- 5) The Chairperson shall perform such other duties as are usually exercised by the Chairperson of a Board or as specifically authorized by the Board.

1.1.b Vice-Chairperson

- 1) The position of Vice-Chairperson is hereby established.
- 2) The Vice-Chairperson shall act in the capacity of Chairperson in the absence of the Chairperson.
- 3) In the event the office of Chairperson becomes vacant, the Vice-Chairperson shall succeed to this office for the unexpired term and the Board shall elect a successor to the office of Vice-Chairperson for the unexpired term.

1.1.c Temporary Chairperson

- 1) In the absence of both the Chairperson and Vice-Chairperson, a temporary Chairperson temporary shall preside over meetings.

1.1.d Secretary

- 1) City staff designated by the Director of the Department of Development & Neighborhood Services shall provide staff support to the Board. The Board may assign the Secretary other duties and responsibilities at its discretion.

- 1.2 Election of Officers and appointment of committees.
 - 1.2.a. The Chairperson of the Board shall be elected at the Annual Meeting of the Board and shall hold office for one year or until his/her successor is elected.
 - 1.2.b The Vice-Chairperson shall be elected at the Annual Meeting of the Board immediately after the election of the Chairperson and shall hold office for one year or until his/her successor is elected.
 - 1.2.c In the absence of both the Chairperson and Vice-Chairperson, a temporary Chairperson shall be elected and shall hold office for the remainder of the meeting.

Section 2.0 Meetings.

- 2.1 Regular Meetings.
 - 2.1.a The Annual Meeting for the election of Officers of the Board shall be held at the January Board meeting each year.
 - 2.1.b The regular monthly meetings of the Board shall be held on Mondays beginning at 5:00 p.m. Meetings are generally the fourth Monday of each month; however, variations may occur due to holidays. The upcoming yearly meeting calendar shall be presented at the December Board meeting each year.
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 - 2.2.a Special Meetings may be called by the Chairperson and held at any time or place.
 - 2.2.b A special meeting may also be called at the request, in writing, of any three or more Board members. If the chairperson fails to comply with such request, said members shall call such meeting.
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 - 2.3.a The Board Secretary shall file with the Deputy City Clerk the time and place of all Board meetings, together with a copy of the Agenda and previous minutes at least twenty-four hours in advance of the meeting.

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 - 2.4.a Roll call of members shall start each Board meeting.
 - 2.4.b The presence of a majority of the official members of the board shall constitute a quorum to legally transact board business.
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- 2.5 Order of Business: Agenda
 - 1) Call to Order
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 - 3) Old Business
 - 4) New Business
 - 5) General Discussion
 - 6) Public Comment
 - 7) Adjournment
- 2.6 Voting.
 - 2.6.a Voting will be by voice roll call, called by the Secretary, and will be recorded by yes, no, or abstain.
 - 2.6.b All members of the Board, including the Chairperson, are required to cast a vote upon each motion.
 - 2.6.c A member may abstain, if the member believes there is a conflict of interest, as provided for in Subsection 3.7.
 - 2.6.d Members shall be physically present at the meeting in order to exercise their vote. Proxy voting, mail voting or telephone voting are prohibited.
 - 2.6.e The affirmative vote of a majority of those members voting shall be required for the exercise of powers or functions conferred or imposed

on the Board, but less than a quorum of the members may meet and adjourn from time to time until a quorum is present.

2.7 Conflict of Interest.

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An appearance of impropriety shall mean an apparent conflict of interest based on objective standards. A member of the Board shall declare their conflict or appearance of impropriety as soon as the matter comes before the Board for discussion or as soon thereafter as the member becomes aware of the appearance of impropriety or conflict. Thereafter, the member shall take no part in the discussion or vote on the matter.

Section 3.0 Meeting Attendance.

3.1 All members of the Board are expected to attend all meetings.

3.2 Prior to any scheduled Board meeting, members shall notify the Board Secretary regarding excused absences. Excused absences include, but not be limited to, death, illness, or being out of the city.

Section 4.0 Communications.

4.1 No member of the Board is qualified to speak for the Board or to give public expression of the opinion, attitude or action of the Board on any matter, unless specifically directed to do so by resolution of the Board. The right of a member of the Board as a citizen to his/her personal opinion, written or spoken, is not hereby denied.

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4.4 Any written communication to a Board member will be reported and presented to the entire Board and shall be included in the public record.

Section 5.0 Committees.

- 5.1 The Chairperson may, from time to time, appoint committees to assist staff in the development of design standards, preparation of planning studies, review of best practices, etc.

Section 6.0 New Board Member Orientation.

- 6.1 Newly appointed Board Members shall be provided the opportunity to meet with the City staff, and other Board members at least one week before their first scheduled regular meeting for purpose of orientation.

Section 7.0 Rules of Order.

- 7.1 Parliamentary Procedure.

Roberts Rules of Order, Revised, will serve as a guide for the Board meetings in all cases where these rules do not provide for the procedures to be followed.

- 7.2 Amendments to bylaws and rules of procedure.

The official bylaws and rules of procedure may be amended at any regular or special meeting.

City of Davenport

Department: Development & Neighborhood Services
Contact Info: Matt Werderitch | 563-888-2221

Action / Date
3/23/2026

Subject:
Election of Vice-Chairperson

Recommendation:
Nominate and elect a Vice-Chairperson

Background:
The Bylaws and Rules of Procedure for the Design Review Board state, "The Vice-Chairperson shall be elected at the Annual Meeting of the Board immediately after the election of the Chairperson and shall hold office for one year or until his/her successor is elected."

The election of a Vice-Chairperson is to take place at the January Board meeting each year.

The following responsibilities apply to the role of Vice-Chairperson:

1. The Vice-Chairperson shall act in the capacity of Chairperson in the absence of the Chairperson.
2. In the event the office of Chairperson becomes vacant, the Vice-Chairperson shall succeed to this office for the unexpired term and the Board shall elect a successor to the office of Vice-Chairperson for the unexpired term.

Attachments:
1. Design Review Board Bylaws

**BYLAWS AND RULES OF PROCEDURE
DESIGN REVIEW BOARD
CITY OF DAVENPORT, IOWA**

Adopted 2-22-2021

Section 1.0 Officers.

1.1 Offices.

1.1.a Chairperson

- 1) The position of Chairperson is hereby established.
- 2) The Chairperson shall preside at all meetings of the Board.
- 3) The Chairperson shall have general charge of the business of the Board.
- 4) The Chairperson shall appoint all committees.
- 5) The Chairperson shall perform such other duties as are usually exercised by the Chairperson of a Board or as specifically authorized by the Board.

1.1.b Vice-Chairperson

- 1) The position of Vice-Chairperson is hereby established.
- 2) The Vice-Chairperson shall act in the capacity of Chairperson in the absence of the Chairperson.
- 3) In the event the office of Chairperson becomes vacant, the Vice-Chairperson shall succeed to this office for the unexpired term and the Board shall elect a successor to the office of Vice-Chairperson for the unexpired term.

1.1.c Temporary Chairperson

- 1) In the absence of both the Chairperson and Vice-Chairperson, a temporary Chairperson temporary shall preside over meetings.

1.1.d Secretary

- 1) City staff designated by the Director of the Department of Development & Neighborhood Services shall provide staff support to the Board. The Board may assign the Secretary other duties and responsibilities at its discretion.

- 1.2 Election of Officers and appointment of committees.
 - 1.2.a. The Chairperson of the Board shall be elected at the Annual Meeting of the Board and shall hold office for one year or until his/her successor is elected.
 - 1.2.b The Vice-Chairperson shall be elected at the Annual Meeting of the Board immediately after the election of the Chairperson and shall hold office for one year or until his/her successor is elected.
 - 1.2.c In the absence of both the Chairperson and Vice-Chairperson, a temporary Chairperson shall be elected and shall hold office for the remainder of the meeting.

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